



CITY OF STANWOOD

103RD DR NW IMPROVEMENTS PROJECT

PHASE II



ATWELL
25 CENTRAL WAY, SUITE 400
KIRKLAND, WA 98033
P: 425.216.4031
WWW.ATWELL-GROUP.COM

SCALE:
AS NOTED

PROJECT MANAGER:
BRETT JUSTINEN, PE

PROJECT ENGINEER:
EVAN BOYARD, EIT

DESIGNER:
CAROL DOGGETT

ISSUE DATE:
4/1/2025

REVISIONS

NO. DATE BY

COVER SHEET
103RD DR NW
IMPROVEMENTS PROJECT
PHASE II
CITY OF STANWOOD WASHINGTON



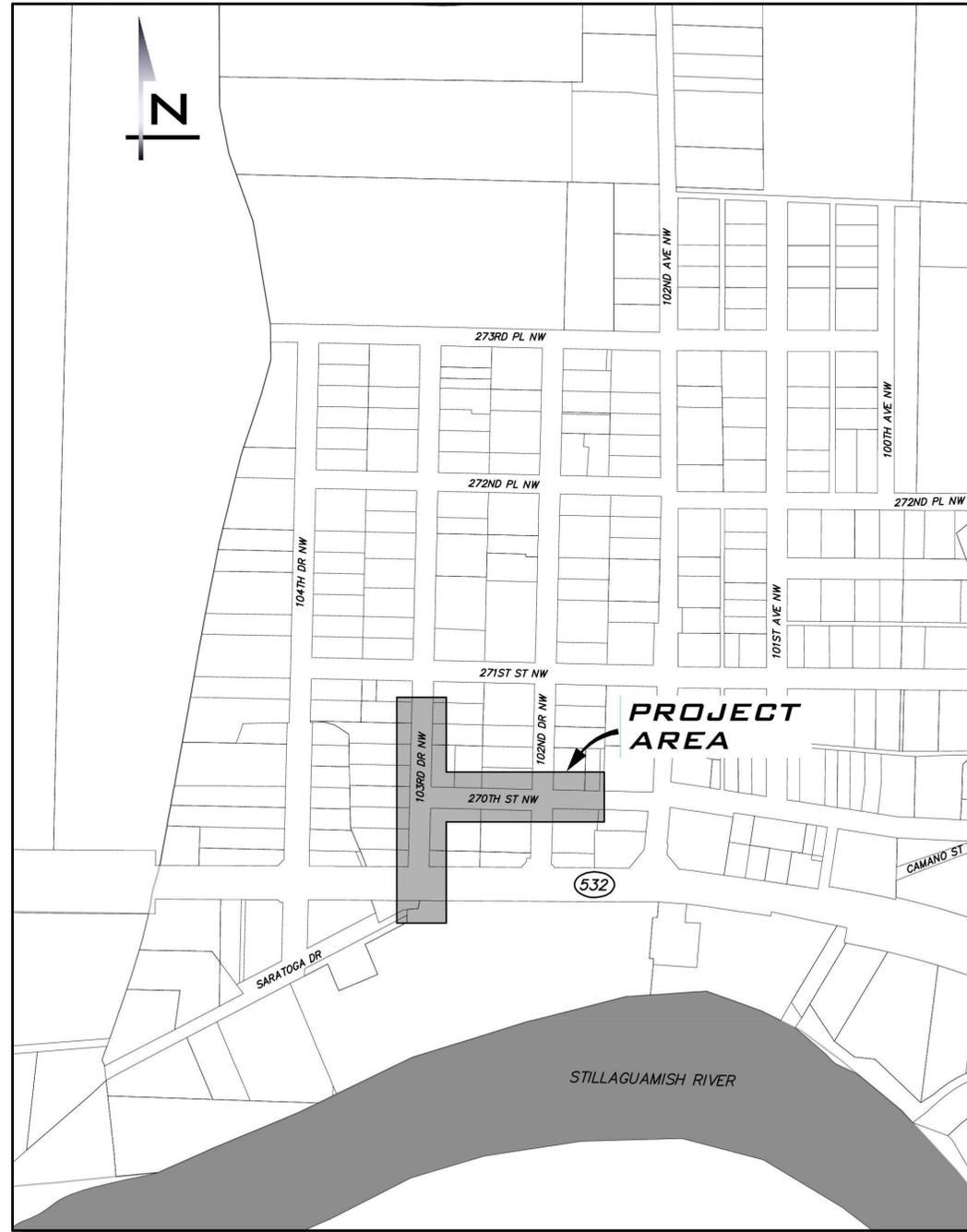
4/1/25

JOB NUMBER:
23006268

SHEET NAME:
CV-01

SHT **1** OF **17**

LEGEND	
PROPOSED FEATURES	
— CURB FLOWLINE	■ MAILBOX
— SIDEWALK	■ SIGN
— CENTER LINE	■ GRAVEL
- - - SAWCUT	■ CONCRETE
— FENCE	■ DETECTABLE WARNING SURFACE
■ HOT-MIX ASPHALT	
■ FULL DEPTH PAVEMENT REPAIR	
PROPOSED UTILITIES	
— WATER MAIN	— GATE VALVE
— WATER METER	— TEE W/ CONC BLOCKING
— IRRIGATION METER	— BEND W/ CONC BLOCKING
— FIRE HYDRANT	— REDUCER
— AIR/VAC RELEASE VALVE	— SLEEVE
— BLOW OFF	— COUPLING
EXISTING FEATURES	
— ADJACENT PLAT/PARCEL LINE	— GUY ANCHOR
— ADJACENT RIGHT-OF-WAY	— POWER METER
— CENTERLINE	— JUNCTION BOX
— SURFACE FEATURES	— STREET LIGHT CONTROL BOX
— BUILDING FOOTPRINT	— GAS METER
— 10' CONTOURS	— TELECOMM VAULT
— 2' CONTOURS	— MAIL BOX
— SD — STORM DRAIN PIPE	— SIGN
— SS — SEWER MAIN	— CONIFEROUS TREE
— W — WATER MAIN	— DECIDUOUS TREE
— AP — AERIAL POWER LINE	— ASPHALT
— B — BURIED POWER LINE	— CONCRETE
— G — GAS MAIN	— GRAVEL
— T — BURIED TELECOMM LINE	
— X — FENCE	
— DITCH OR SWALE	
— RETAINING WALL	
□ CATCH BASIN, TYPE I	
□ CATCH BASIN, TYPE II	
○ SD PIPE FLOW	
○ STORM CLEANOUT	
○ SEWER MANHOLE	
○ FIRE HYDRANT	
○ WATER METER	
○ GATE VALVE	
○ UP (UTILITY POLE)	
○ UP W/ TRANSFORMER	
○ UP W/ UNDERGROUND CONDUIT	
○ UP W/ TRANSFORMER & UNDERGROUND CONDUIT	
○ STREET LIGHT	
○ STREET LIGHT W/ TRANSFORMER	
○ STREET LIGHT W/ TRANSFORMER & UNDERGROUND CONDUIT	



VICINITY MAP
SCALE: 1" = 200'

PROJECT TEAM

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EXISTING UTILITY NOTE

EXISTING UTILITIES ARE SHOWN IN THE APPROXIMATE LOCATION. THERE IS NO GUARANTEE THAT ALL UTILITY LINES ARE SHOWN, OR THAT THE LOCATION, SIZE AND MATERIAL IS ACCURATE. THE CONTRACTOR SHALL UNCOVER ALL INDICATED PIPING WHERE CROSSING, INTERFERENCES, OR CONNECTIONS OCCUR PRIOR TO TRENCHING OR EXCAVATION FOR ANY PIPE OR STRUCTURES, TO DETERMINE ACTUAL LOCATIONS, SIZE AND MATERIAL. THE CONTRACTOR SHALL MAKE THE APPROPRIATE PROVISION FOR PROTECTION OF SAID FACILITIES. THE CONTRACTOR SHALL NOTIFY ONE CALL AT 8-1-1 (WASHINGTON811.COM) AND ARRANGE FOR FIELD LOCATION OF EXISTING FACILITIES BEFORE CONSTRUCTION.

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STREET CONSTRUCTION GENERAL NOTES

- ALL WORKMANSHIP AND MATERIALS SHALL COMPLY WITH THE STANWOOD STREET AND UTILITY STANDARDS AND STANDARD DETAILS AND THE MOST CURRENT COPY OF THE STATE OF WASHINGTON STANDARD SPECIFICATIONS FOR ROAD, BRIDGE AND MUNICIPAL CONSTRUCTION.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL TRAFFIC CONTROL IN ACCORDANCE WITH MUTCD. PRIOR TO DISRUPTION OF ANY TRAFFIC, TRAFFIC CONTROL PLANS SHALL BE PREPARED AND SUBMITTED TO THE CITY FOR APPROVAL. NO WORK SHALL COMMENCE UNTIL ALL APPROVED TRAFFIC CONTROL IS IN PLACE.
- A LICENSED ENGINEERING OR SURVEYING FIRM SHALL STAKE ALL CURB AND GUTTER, STREET GRADES, SIDEWALK GRADES AND ANY OTHER VERTICAL AND/OR HORIZONTAL ALIGNMENT.
- WHERE NEW ASPHALT JOINS EXISTING, THE EXISTING ASPHALT SHALL BE CUT TO A NEAT VERTICAL EDGE AND TACKED WITH ASPHALT EMULSION TYPE, CSS-1, IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS. THE NEW ASPHALT SHALL BE FEATHERED BACK OVER EXISTING TO PROVIDE FOR A SEAL AT THE SAW CUT LOCATION AND THE JOINT SEALED WITH GRADE AR 4000W PAVING ASPHALT.
- COMPACTION OF SUBGRADE, ROCK AND ASPHALT SHALL BE IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS.
- FORM AND SUBGRADE INSPECTION BY THE CITY IS REQUIRED BEFORE POURING CONCRETE. TWENTY-FOUR HOURS NOTICE IS REQUIRED FOR FORM INSPECTION.
- SEE THE STANWOOD STREET AND UTILITY STANDARDS CHAPTER 2 FOR TESTING AND SAMPLING FREQUENCIES.
- PRIOR TO ISSUANCE OF A CERTIFICATE OF OCCUPANCY, THE DEVELOPER SHALL PROVIDE AND INSTALL ALL STREET NAME, REGULATORY, WARNING AND GUIDE SIGNS.

WATER MAIN CONSTRUCTION GENERAL NOTES

- ALL WORKMANSHIP AND MATERIAL SHALL BE IN ACCORDANCE WITH CITY OF STANWOOD STANDARDS AND THE MOST CURRENT COPY OF THE STATE OF WASHINGTON STANDARD SPECIFICATIONS FOR ROAD, BRIDGE, AWWA STANDARDS, APWA AND MUNICIPAL CONSTRUCTION.
- A PRECONSTRUCTION MEETING SHALL BE HELD WITH THE CITY PRIOR TO THE START OF CONSTRUCTION.
- WATER MAINS SHALL BE PVC PIPE CLASS C900.
- GATE VALVES SHALL BE RESILIENT WEDGE, NRS (NON RISING STEM) WITH O-RING SEALS. VALVE ENDS SHALL BE MECHANICAL JOINT OR ANSI FLANGES. GATE VALVES SHALL HAVE STAINLESS STEEL BONNET AND Q-LAND BOLTS. GATE VALVES SHALL HAVE ELECTROSTATICALLY APPLIED FUSION-BONDED EPOXY-RESIN COATING MEETING OR EXCEEDING AWWA C550. VALVES SHALL CONFORM TO AWWA C509 OR C515. GATE VALVES SHALL BE MUELLER, M & H, AVK, OR WATEROUS. EXISTING VALVES TO BE OPERATED BY CITY EMPLOYEES ONLY.
- HYDRANTS SHALL BE M & H RELIANT STYLE 929, OR CLOW F-2500 OR WATEROUS PACER. HYDRANTS SHALL BE BAGGED UNTIL SYSTEM IS APPROVED. HYDRANTS WILL COME COMPLETE WITH STORZ ADAPTERS.
- ALL LINES SHALL BE CHLORINATED AND TESTED IN CONFORMANCE WITH THE ABOVE REFERENCED SPECIFICATION (NOTE 1).
- ALL WATER PIPES AND SERVICES SHALL BE INSTALLED WITH DETECTABLE MARKING TAPE INSTALLED 18" ABOVE THE PIPE CROWN, OR 12" BELOW FINISHED GRADE (WHICHEVER IS DEEPER). DETECTABLE MARKING TAPE SHALL CONFORM TO WSDOT/APWA STANDARD SPECIFICATIONS. IN ADDITION, ALL NON-METALLIC PIPES AND SERVICES SHALL BE INSTALLED WITH 14 GAUGE COATED COPPER WIRE WRAPPED AROUND THE PIPE, BROUGHT UP WITH THREE FEET OF LOOSE WIRE AND TIED OFF AT VALVE BODY, METER BOX OR AS DIRECTED BY THE INSPECTOR. THE CONTRACTOR SHALL FURNISH AND INSTALL THE TAPE AND WIRE.
- PROVIDE TRAFFIC CONTROL PLAN(S) AS REQUIRED IN ACCORDANCE WITH MUTCD.
- ALL WATER MAINS SHALL BE STAKED FOR GRADES AND ALIGNMENT BY AN ENGINEERING OR SURVEYING FIRM CAPABLE OF PERFORMING SUCH WORK.
- ALL EXISTING CEMENT ASBESTOS PIPES SHALL BE HANDLED AND DISPOSED OF ACCORDING TO STATE AND FEDERAL STATUTES.
- CALL UNDERGROUND LOCATE AT 1-800-424-5555 A MINIMUM OF 48 HOURS PRIOR TO ANY EXCAVATIONS.
- THE CITY WILL BE GIVEN 72 HOURS NOTICE PRIOR TO SCHEDULING A SHUTDOWN. SHUTDOWNS SHALL NOT OCCUR ON MONDAYS, FRIDAYS, CITY HOLIDAYS, OR THE DAY BEFORE OR AFTER A CITY HOLIDAY. WHERE CONNECTIONS REQUIRE FIELD VERIFICATION, CONNECTION POINTS WILL BE EXPOSED BY CONTRACTOR AND FITTINGS VERIFIED 48 HOURS PRIOR TO DISTRIBUTING SHUTDOWN NOTICES.
- ALL WATER PIPE AND APPURTENANCES SHALL BE LEAD FREE IN ACCORDANCE WITH THE SAFE DRINKING WATER ACT, SECTION 1417.

ESC GENERAL NOTES

- APPROVAL OF THIS EROSION/SEDIMENTATION CONTROL (ESC) PLAN DOES NOT CONSTITUTE AN APPROVAL OF PERMANENT ROAD OR DRAINAGE DESIGN (E.G. SIZE AND LOCATION OF ROADS, PIPES, RESTRICTORS, CHANNELS, RETENTION FACILITIES, UTILITIES, ETC).
- THE IMPLEMENTATION OF THESE ESC PLANS AND THE CONSTRUCTION, MAINTENANCE, REPLACEMENT, AND UPGRADING OF THESE ESC FACILITIES IS THE RESPONSIBILITY OF THE CONTRACTOR UNTIL ALL CONSTRUCTION IS COMPLETED AND APPROVED AND VEGETATION/ LANDSCAPING IS ESTABLISHED.
- THE BOUNDARIES OF THE CLEARING LIMITS SHOWN ON THIS PLAN SHALL BE CLEARLY MARKED IN THE FIELD PRIOR TO CONSTRUCTION. DURING THE CONSTRUCTION PERIOD, NO DISTURBANCE BEYOND THE MARKED CLEARING LIMITS SHALL BE PERMITTED. THE MARKING SHALL BE MAINTAINED BY THE CONTRACTOR FOR THE DURATION OF CONSTRUCTION.
- THE ESC FACILITIES SHOWN ON THIS PLAN MUST BE CONSTRUCTED IN CONJUNCTION WITH ALL CLEARING AND GRADING ACTIVITIES, AND IN SUCH A MANNER AS TO ENSURE THAT SEDIMENT LADEN WATER DO NOT ENTER THE DRAINAGE SYSTEM, ROADWAYS OR VIOLATE APPLICABLE WATER & UTILITY STANDARDS.
- THE ESC FACILITIES SHOWN ON THIS PLAN ARE THE MINIMUM REQUIREMENTS FOR ANTICIPATED SITE CONDITIONS. DURING THE CONSTRUCTION PERIOD, THESE ESC FACILITIES SHALL BE UPGRADED AS NEEDED FOR UNEXPECTED STORM EVENTS AND TO ENSURE THAT SEDIMENT AND SEDIMENT-LADEN WATER DO NOT LEAVE THE SITE.
- THE ESC FACILITIES SHALL BE INSPECTED DAILY BY THE CONTRACTOR AND MAINTAINED AS NECESSARY TO ENSURE THEIR CONTINUED FUNCTIONING.
- THE ESC FACILITIES ON INACTIVE SITES SHALL BE INSPECTED AND MAINTAINED A MINIMUM OF ONCE A MONTH OR WITHIN THE 48 HOURS FOLLOWING A MAJOR STORM EVENT.
- AT NO TIME SHALL MORE THAN 1" OF SEDIMENT BE ALLOWED TO ACCUMULATE WITHIN THESUMP OF A TRAPPED CATCH BASIN. ALL CATCH BASINS AND CONVEYANCE LINES SHALL BE CLEANED PRIOR TO PAVING. THE CLEANING OPERATION SHALL NOT FLUSH SEDIMENT LADEN WATER INTO THE DOWNSTREAM SYSTEM.
- ADDITIONAL MEASURES MAY BE REQUIRED TO ENSURE THAT ALL PAVED AREAS ARE KEPT CLEAN FOR THE DURATION OF THE PROJECT. STREETS MUST BE KEPT CLEAN AT ALL TIMES WHEN NOT BEING WORKED.

LEGAL NOTES

- ALL LABOR, MATERIALS, WORKMANSHIP, AND METHODS OF CONSTRUCTION SHALL BE IN STRICT ACCORDANCE WITH THE MINIMUM ENGINEERING AND CONSTRUCTION STANDARDS OF ANY AND ALL APPLICABLE AGENCIES, PERMITTING AUTHORITIES, AND OTHER GOVERNMENTAL ENTITIES, AND THE PLANS. WHERE CONFLICTS OR OMISSIONS EXIST, THE MORE STRINGENT OF THE PLANS OR APPLICABLE JURISDICTIONAL STANDARDS SHALL DICATE. CONTRACTOR SHALL BRING ALL CONFLICTS TO THE ATTENTION OF THE ENGINEER PRIOR TO PROCEEDING WITH ANY WORK RELATED TO SUCH CONFLICT. SUBSTITUTION AND/OR DEVIATION FROM THE PLANS AND SPECIFICATIONS SHALL BE PERMITTED ONLY WHEN WRITTEN APPROVAL HAS BEEN ISSUED BY THE ENGINEER.
- THIS PLAN IS NOT VALID UNLESS IT BEARS THE SIGNATURE AND SEAL OF THE LICENSED PROFESSIONAL IDENTIFIED IN THIS PLAN. THIS PLAN IS NOT VALID IF IT IS COPIED, OR IF IT IS WITHOUT THE ORIGINAL SIGNATURE AND SEAL OF THE LICENSED PROFESSIONAL SHOWN HEREON.
- THIS PLAN WAS PREPARED FOR THE EXCLUSIVE USE OF THE PERSON, PERSONS, OR ENTITY NAMED HEREON. THIS PLAN DOES NOT EXTEND TO ANY UNNAMED PERSON, PERSONS OR ENTITY WITHOUT EXPRESS WRITTEN CERTIFICATION BY THE LICENSED PROFESSIONAL NAMING SAID PERSON, PERSONS, OR ENTITY.
- THE LICENSED PROFESSIONAL SHALL NOT BE HELD LIABLE FOR ANY THIRD PARTY'S USE OR INTERPRETATION OF THE DRAWINGS OR REVISIONS TO SAME WITHOUT THE APPROVAL OF THE LICENSED PROFESSIONAL.
- CONTRACTOR SHALL NOT RELY SOLELY ON ELECTRONIC VERSIONS OF PLANS, SPECIFICATIONS, AND DATA FILES THAT ARE OBTAINED FROM THE DESIGNER, BUT SHALL VERIFY LOCATIONS OF PROJECT FEATURES IN ACCORDANCE WITH THE SEALED PAPER COPIES OF THE PLANS AND SPECIFICATIONS THAT ARE SUPPLIED AS PART OF THE CONTRACT DOCUMENTS.
- CONSTRUCTION SITE SAFETY IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. NEITHER THE OWNER NOR THE ENGINEER SHALL BE EXPECTED TO ASSUME ANY RESPONSIBILITY FOR SAFETY OF THE WORK, OF PERSONS ENGAGED IN THE WORK, OF ANY NEARBY STRUCTURES, OR OF ANY OTHER PERSONS.
- THE CONTRACTOR SHALL HAVE SOLE RESPONSIBILITY FOR THE CONSTRUCTION MEANS, METHODS, AND TECHNIQUES OF EXECUTING ITS WORK, INCLUDING SAFETY.
- CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE CAUSED BY CONTRACTOR'S ACTS OR OMISSIONS AND SHALL MAKE REPAIRS AS NECESSARY TO THE SATISFACTION OF THE OWNER AND APPLICABLE JURISDICTION.
- CONTRACTOR SHALL FIELD VERIFY ALL DIMENSIONS BEFORE CONSTRUCTION. ANY DISCREPANCIES BETWEEN CONSTRUCTION DOCUMENTS AND FIELD CONDITIONS SHALL IMMEDIATELY BE BROUGHT TO THE ATTENTION OF THE OWNER. ANY WORK DONE WITHOUT VERIFICATION IS DONE SO AT THE CONTRACTORS RISK AND EXPENSE IF ERRORS OCCUR.
- THE EXISTING UTILITIES SHOWN ON THE PLANS HAVE BEEN LOCATED BASED ON THE INFORMATION AVAILABLE AND ARE GIVEN FOR THE CONVENIENCE OF THE CONTRACTOR. THE PROFESSIONAL ASSUMES NO RESPONSIBILITY FOR THEIR ACCURACY. CONTRACTOR SHALL PROMPTLY CONTACT THE APPLICABLE ONE CALL AUTHORITY PRIOR TO BEGINNING EXCAVATION TO DETERMINE THE LOCATION OF EXISTING FACILITIES. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK, AND AGREES TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH OCCUR TO THE EXTENT OF CONTRACTOR'S FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES.
- THE CONTRACTOR SHALL CONFORM TO THE LATEST EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS (MUTCD), THE STATE DEPARTMENT OF TRANSPORTATION ROADWAY AND TRAFFIC DESIGN STANDARD INDEXES, AND MUNICIPAL STANDARDS, IF APPLICABLE, PRIOR TO START OF CONSTRUCTION WITHIN ANY RIGHT-OF-WAY THE CONTRACTOR SHALL PROVIDE A FULL MAINTENANCE OF TRAFFIC PLAN TO THE APPROPRIATE JURISDICTION FOR REVIEW AND APPROVAL. PRIOR TO START OF CONSTRUCTION, THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN ALL BARRICADES, WARNING SIGNS, AND MARKINGS FOR HAZARDS AND THE CONTROL OF TRAFFIC ALONG ALL EXISTING ROADWAYS.
- ALL CONSTRUCTION AND MATERIALS SHALL BE IN ACCORDANCE WITH THE PROJECT SPECIFICATIONS, STANDARD DETAILS, AND ANY APPLICABLE LAWS, REGULATIONS, AND STANDARDS ISSUED BY THE CONTROLLING AGENCY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO ENSURE THAT ALL REQUIRED PERMITS ARE OBTAINED AND IN HAND BEFORE BEGINNING ANY CONSTRUCTION. ANY PENALTIES, STOP WORK ORDERS OR ADDITIONAL WORK RESULTING FROM THE CONTRACTOR BEING IN VIOLATION OF THE REQUIREMENTS ABOVE SHALL BE FULLY BORNE BY THE CONTRACTOR.

SURVEY NOTES

EQUIPMENT NOTES

PRIMARY CONTROL POINTS AND ACCESSIBLE MONUMENT POSITIONS WERE FIELD MEASURED UTILIZING GLOBAL POSITIONING SYSTEM (GPS) SURVEY TECHNIQUES USING LEICA GS14 GPS/GNSS EQUIPMENT. MONUMENT POSITIONS THAT WERE NOT DIRECTLY OBSERVED USING GPS SURVEY TECHNIQUES WERE TIED INTO THE CONTROL POINTS UTILIZING LEICA ELECTRONIC TS16 TOTAL STATIONS FOR THE MEASUREMENT OF BOTH ANGLES AND DISTANCES. THIS SURVEY MEETS OR EXCEEDS THE STANDARDS SET BY WACS 332-130-080/090.

NOTES

THE INFORMATION DEPICTED ON THIS MAP REPRESENTS THE RESULTS OF A SURVEY MADE ON JANUARY 15, 2024 AND CAN ONLY BE CONSIDERED AS INDICATING THE GENERAL CONDITIONS EXISTING AT THAT TIME.

UTILITY LOCATIONS SHOWN HEREON ARE BASED UPON ASBUILT FIELD LOCATION OF EXISTING STRUCTURES. FIELD LOCATION OF UTILITIES BASED ON LOCATOR PAINT MARKINGS AND LOCATIONS BASED ON UTILITY MAPS FROM CITY AND UTILITY DRAWINGS INDICATING REPORTED UTILITY INSTALLATIONS. OTHER UTILITIES MAY EXIST. NO SUB-SURFACE EXPLORATION WAS MADE TO VERIFY UTILITY ROUTINGS AND THE ROUTING OF ALL BURIED UTILITIES SHOULD BE CONFIRMED WITH THE UTILITY PURVEYOR AND EXPOSED IN AREAS CRITICAL TO DESIGN FOR VERIFICATION.

1' CONTOURS INTERVAL DERIVED FROM DIRECT FIELD OBSERVATION.

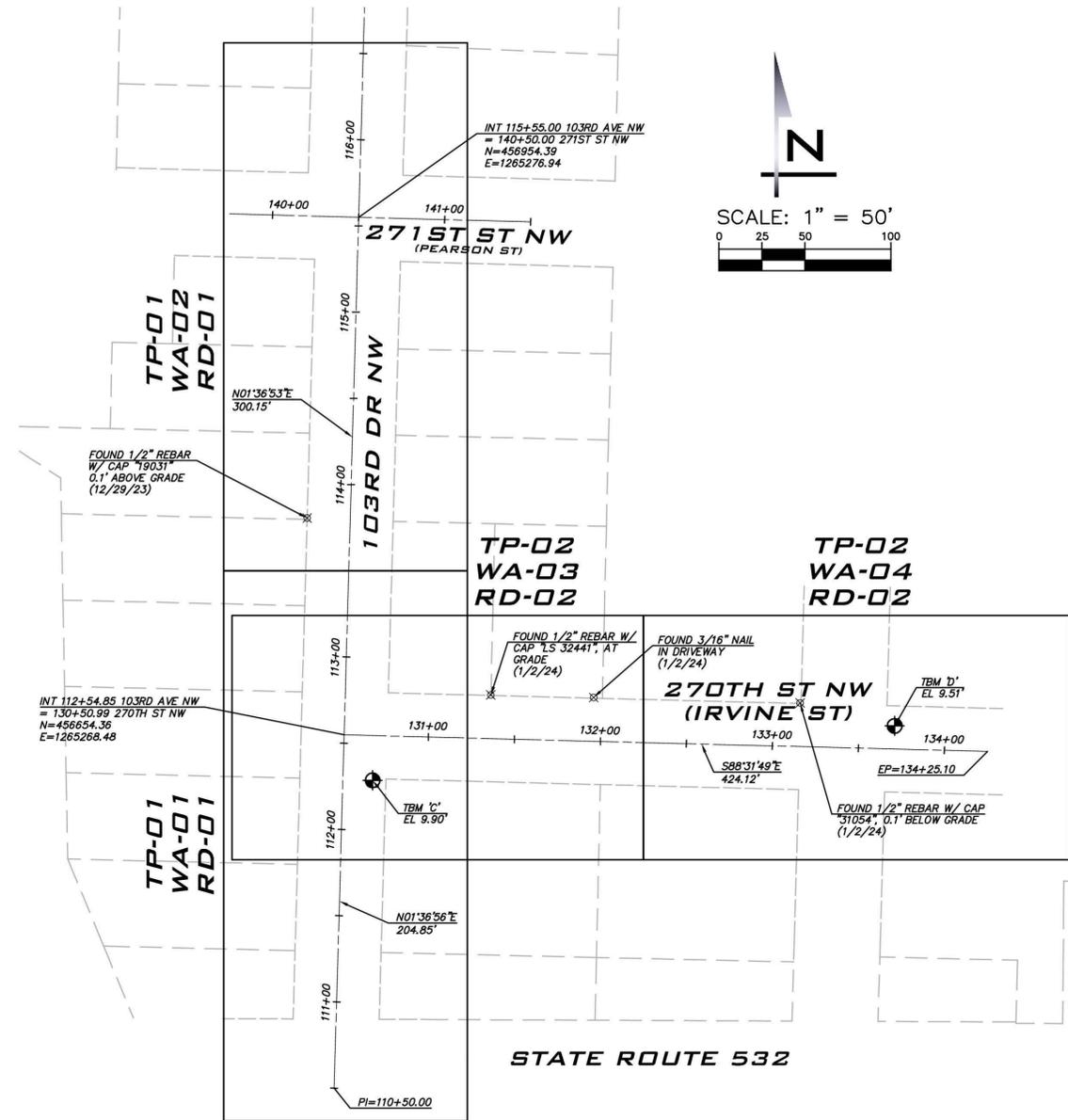
THIS SURVEY MEETS UNITED STATES NATIONAL MAP ACCURACY STANDARDS FOR VERTICAL ACCURACY OF ONE HALF THE CONTOUR INTERVAL.

"NO LOST SIGNAL" NOTE

UTILITY LOCATORS TRACE THROUGH UTILITY SIGNAL CONDUCTIVITY. WHEN CONDUCTOR LEADS ARE SEVERED OR OBSCURED, SIGNALS FAIL TO INDICATE LOCATION. THESE NOTES SHOW THE EXTENT OF CONDUCTIVITY REGARDLESS OF UTILITY TYPE.

GIS PARCEL NOTE

PARCEL LINES AS SHOWN ARE FROM SNOHOMISH COUNTY GIS INFORMATION. NOTE THAT IT IS ONLY A SCHEMATIC REPRESENTATION FOR CONVENIENCE IN INTERPRETING THE TOPOGRAPHY. IT IS NOT A BOUNDARY SURVEY AND SHOULD NOT BE RELIED ON AS SUCH.



DATUM & BENCHMARKS

HORIZONTAL DATUM

NAD 83/91 PER GPS HxGN SMARTNET AS VERIFIED TO SNOHOMISH COUNTY WGS MONUMENT "SW01".

VERTICAL DATUM

NAVD 88 PER GPS HxGN SMARTNET AS VERIFIED TO SNOHOMISH COUNTY WGS MONUMENT "SW01".

BENCHMARKS

ORIGINATING BENCHMARK: SNOHOMISH COUNTY WGS MONUMENT "SW01", FOUND 2" BRASS DISK IN CASE, 0.25' BELOW RIM, AT THE INTERSECTION OF 102ND AVE NW & 270TH ST NW

ELEVATION: 8.86

TEMPORARY BENCHMARKS:

- TBM 'C'**: SET RAILROAD SPIKE IN THE WESTERLY FACE OF A UTILITY POLE WITH LIGHT LOCATED AT THE SOUTHEAST QUADRANT OF 103RD DR NW & 270TH ST NW. ELEVATION: 9.90'
- TBM 'D'**: SET CHISELED SQUARE AT THE TOP BACK OF CURB AT THE NORTHEAST QUADRANT OF 270TH ST NW & 102ND DR NW, ±6" SOUTHWESTERLY OF A SIGN THAT READS "PUBLIC PARKING". ELEVATION: 9.51'

EXISTING UTILITY NOTE

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PROJECT ENGINEER:
EVAN BOVARD, EIT
DESIGNER:
CAROL DOGGETT
ISSUE DATE:
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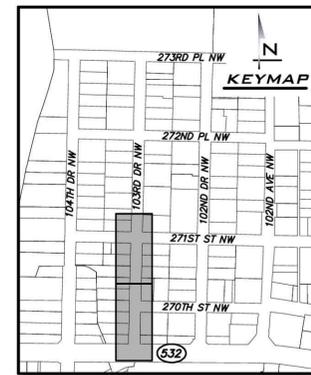
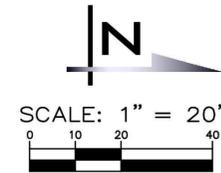
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GENERAL NOTES & KEY MAP
103RD DR NW
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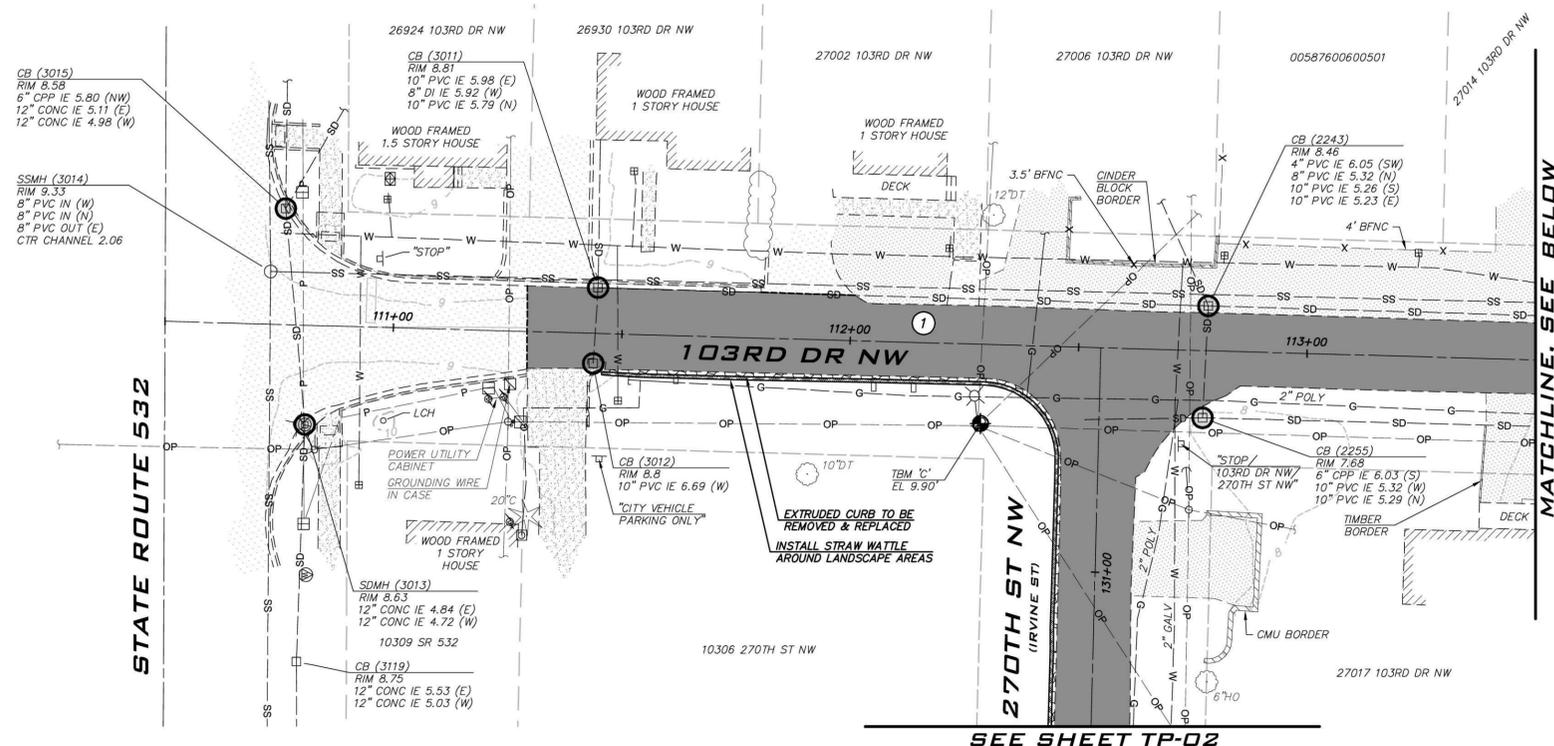
JUSTIN LEE
STATE OF WASHINGTON
REGISTERED PROFESSIONAL ENGINEER
23003857

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SHEET NAME:
GN-01

2/23/24
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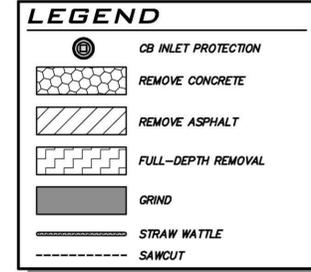
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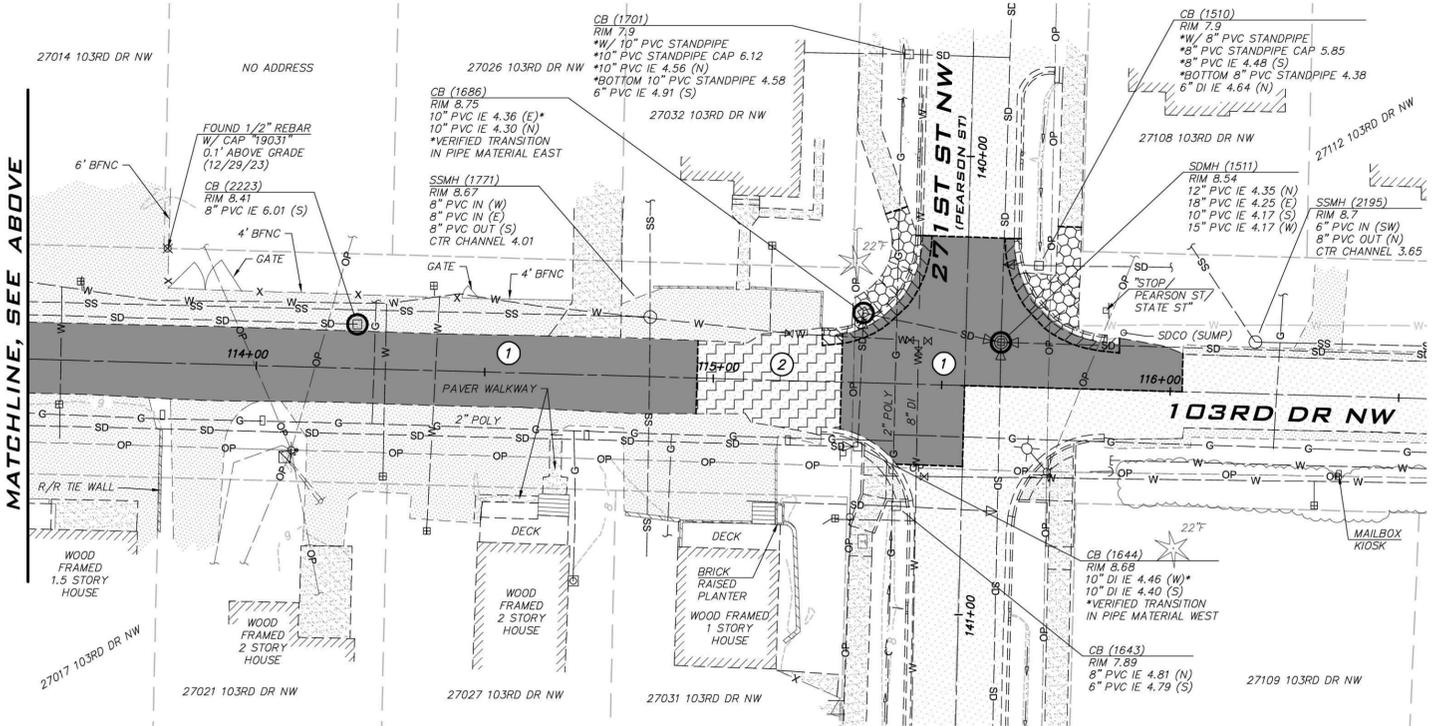
STATE ROUTE 532

MATCHLINE, SEE BELOW

SEE SHEET TP-02



NO	DATE	BY	REVISIONS



MATCHLINE, SEE ABOVE

- NOTES**
- PROTECT EXISTING FENCES, ROCKERIES, WALLS AND OTHER IMPROVEMENTS TO THE MAXIMUM EXTENT FEASIBLE UNLESS OTHERWISE NOTED.
 - FULL-DEPTH REMOVAL LIMITS SHOWN ARE APPROXIMATE. THE CONTRACTOR SHALL OBTAIN APPROVAL FROM THE ENGINEER BEFORE SAWCUTTING.
 - CONTRACTOR TO PROVIDE GAPS IN SILT FENCE FOR PEDESTRIAN AND VEHICLE ACCESS INCLUDING SIDEWALKS, DRIVEWAYS, AND DESIGNATED PARKING AREAS.

- CONSTRUCTION NOTES**
- GRIND EXISTING ASPHALT PAVEMENT TO EXPOSE PCC PANELS. GRIND DEPTH VARIES FROM 1" TO 3" APPROXIMATELY.
 - ROADWAY EXCAVATION. REMOVE TO THE DEPTH SHOWN TO REBUILD PER FULL-DEPTH RECONSTRUCTION SECTION DETAILS ON SHEET DT-01. REMOVAL INCLUDES APPROXIMATELY 4" THICK PORTLAND CEMENT CONCRETE PANELS UNDER A THIN LAYER OF ASPHALT PAVEMENT.

EXISTING UTILITY NOTE

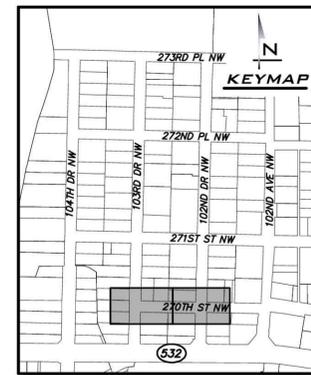
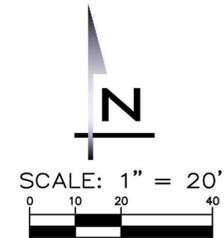
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EXISTING CONDITIONS, DEMO & TESC PLAN
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 CITY OF STANWOOD WASHINGTON



4/1/25
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TP-01

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SCALE:
AS NOTED

PROJECT MANAGER:
BRETT JUSTINEN, PE

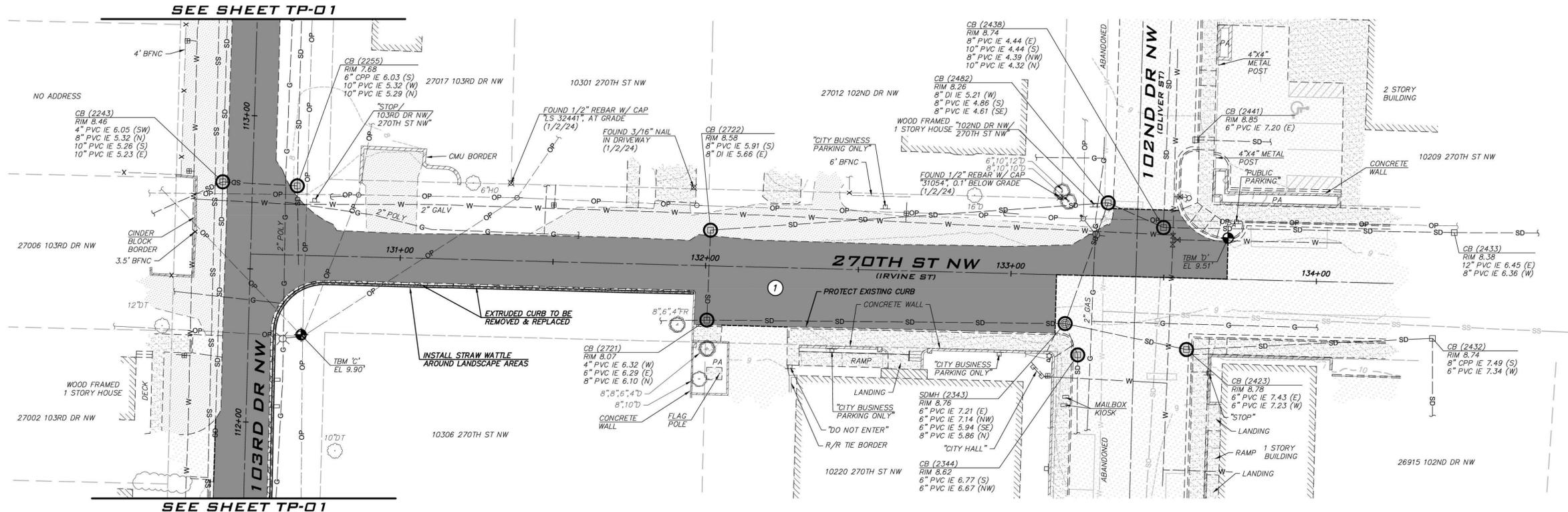
PROJECT ENGINEER:
EVAN BOYARD, EIT

DESIGNER:
CAROL DOGGETT

ISSUE DATE:
4/1/2025

LEGEND

- CB INLET PROTECTION
- REMOVE CONCRETE
- REMOVE ASPHALT
- FULL-DEPTH REMOVAL
- GRIND
- STRAW WATTLE
- SAWCUT



NO	DATE	BY	REVISIONS

EXISTING CONDITIONS, DEMO & TESC PLAN
 103RD DR NW
 IMPROVEMENTS PROJECT
 PHASE II
 CITY OF STANWOOD WASHINGTON

- NOTES**
- PROTECT EXISTING FENCES, ROCKERIES, WALLS AND OTHER IMPROVEMENTS TO THE MAXIMUM EXTENT FEASIBLE UNLESS OTHERWISE NOTED.
 - FULL-DEPTH REMOVAL LIMITS SHOWN ARE APPROXIMATE. THE CONTRACTOR SHALL OBTAIN APPROVAL FROM THE ENGINEER BEFORE SAWCUTTING.
 - CONTRACTOR TO PROVIDE GAPS IN SILT FENCE FOR PEDESTRIAN AND VEHICLE ACCESS INCLUDING SIDEWALKS, DRIVEWAYS, AND DESIGNATED PARKING AREAS.

- CONSTRUCTION NOTES**
- GRIND EXISTING ASPHALT PAVEMENT TO EXPOSE PCC PANELS. GRIND DEPTH VARIES FROM 1" TO 3" APPROXIMATELY.
 - ROADWAY EXCAVATION. REMOVE TO THE DEPTH SHOWN TO REBUILD PER FULL-DEPTH RECONSTRUCTION SECTION DETAILS ON SHEET DT-01. REMOVAL INCLUDES APPROXIMATELY 4" THICK PORTLAND CEMENT CONCRETE PANELS UNDER A THIN LAYER OF ASPHALT PAVEMENT.

EXISTING UTILITY NOTE

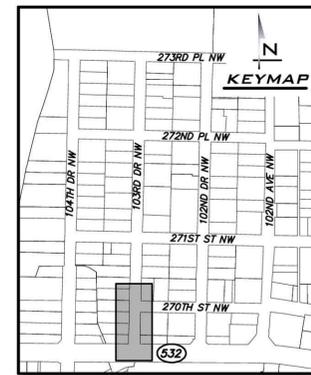
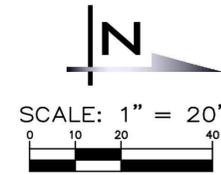
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4/1/25

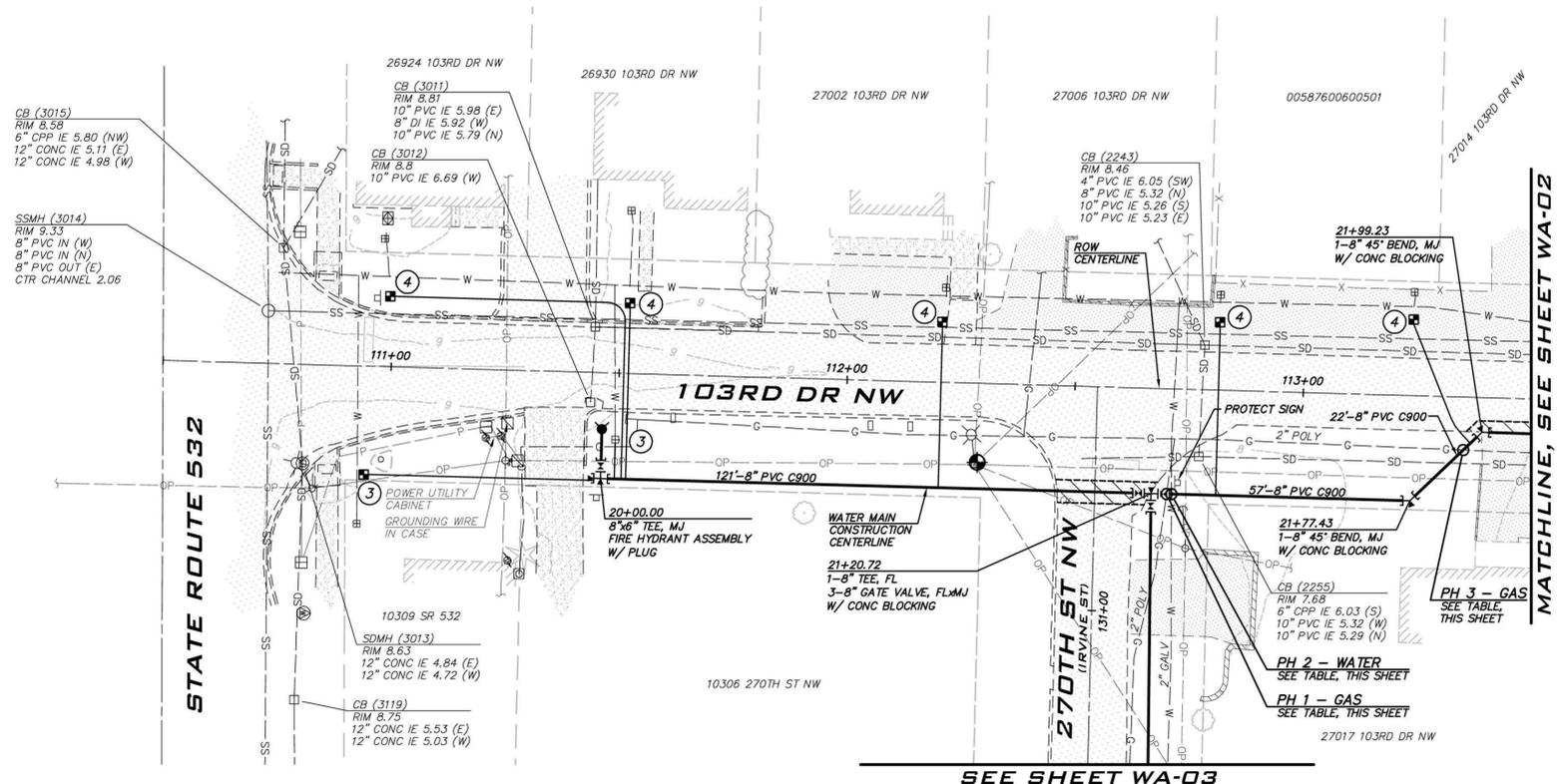
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 BRETT JUSTINEN, PE
 PROJECT ENGINEER:
 EVAN BOYARD, EIT
 DESIGNER:
 CAROL DOGGETT
 ISSUE DATE:
 4/1/2025



LEGEND

- WATER MAIN
- - - SAWCUT
- ▨ FULL DEPTH RESTORATION TRENCH PATCH

STATE ROUTE 532

MATCHLINE, SEE SHEET WA-02

SEE SHEET WA-03

POTHOLE DATA

POTHOLE #	TARGET UTILITY	PIPE CONDUIT SIZE (INCHES)	PIPE MATERIAL	DEPTH TO TOP OF UTILITY (INCHES)	DEPTH TO BOTTOM OF UTILITY (INCHES)	ASPHALT THICKNESS
1	GAS	2"	POLY	28"	30"	ASSUMED 2"
2	WATER	2"	GALV	20"	22"	ASSUMED 2"
3	GAS	2"	POLY	33"	35"	ASSUMED 2"

CONSTRUCTION NOTES

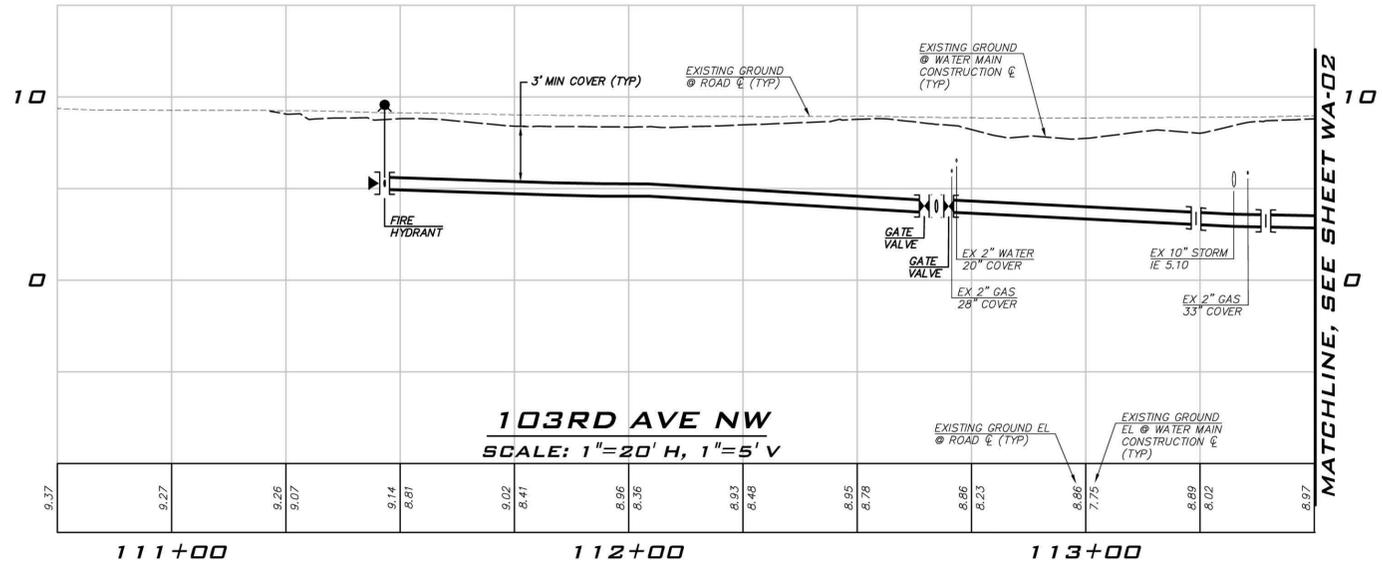
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- INSTALL LIVE TAP PER COS STD DRAWING W-9.
- INSTALL PRESSURE RATED CASING ON WATER MAIN WITH ETHAFOAM PADDING AT ALL SANITARY SEWER CROSSINGS PER DETAIL, SHEET DT-04. CASINGS SHALL EXTEND 10' BOTH SIDES FROM CENTER OF SANITARY SEWER PIPE.
- INSTALL 2" BLOWOFF PER COS STD DRAWING W-17.
- INSTALL PRESSURE RATED CASING ON WATER MAIN WITH ETHAFOAM PADDING.

NOTES

- ALL FITTINGS TO BE INSTALLED WITH STAINLESS STEEL BOLTS.
- REPLACE DISTURBED GRAVEL TO EXISTING CONDITIONS OR BETTER.
- 32#D5 ULTRAMAG HIGH POTENTIAL MAGNESIUM ANODES TO BE INSTALLED AT ALL FITTINGS PER COS STD DRAWING W-16.
- TRENCH RESTORATION OUTSIDE OF THE ROADWAY SHALL BE BACKFILLED PER WATER TRENCH SECTION DETAIL ON SHEET DT-01. THE TOP 4 INCHES SHALL BE RESTORED WITH TYPE A TOPSOIL AND HYDROSEEDED OR SOD PLACED (AT THE OPTION OF THE PROPERTY OWNER) FOR LAWN RESTORATION AND THE TOP 4 INCHES SHALL BE RESTORED WITH CSTC FOR GRAVEL PARKING, SHOULDER, AND DRIVEWAY AREAS. OTHERWISE, RESTORE AREAS TO EXISTING CONDITIONS OR BETTER.
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EXISTING UTILITY NOTE

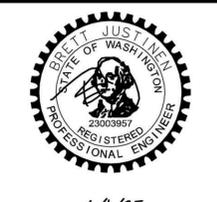
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MATCHLINE, SEE SHEET WA-02

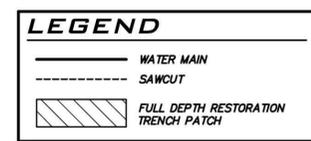
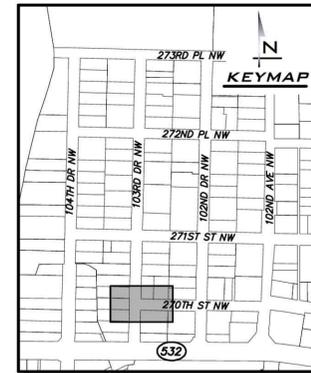
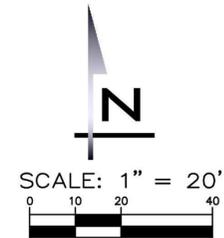
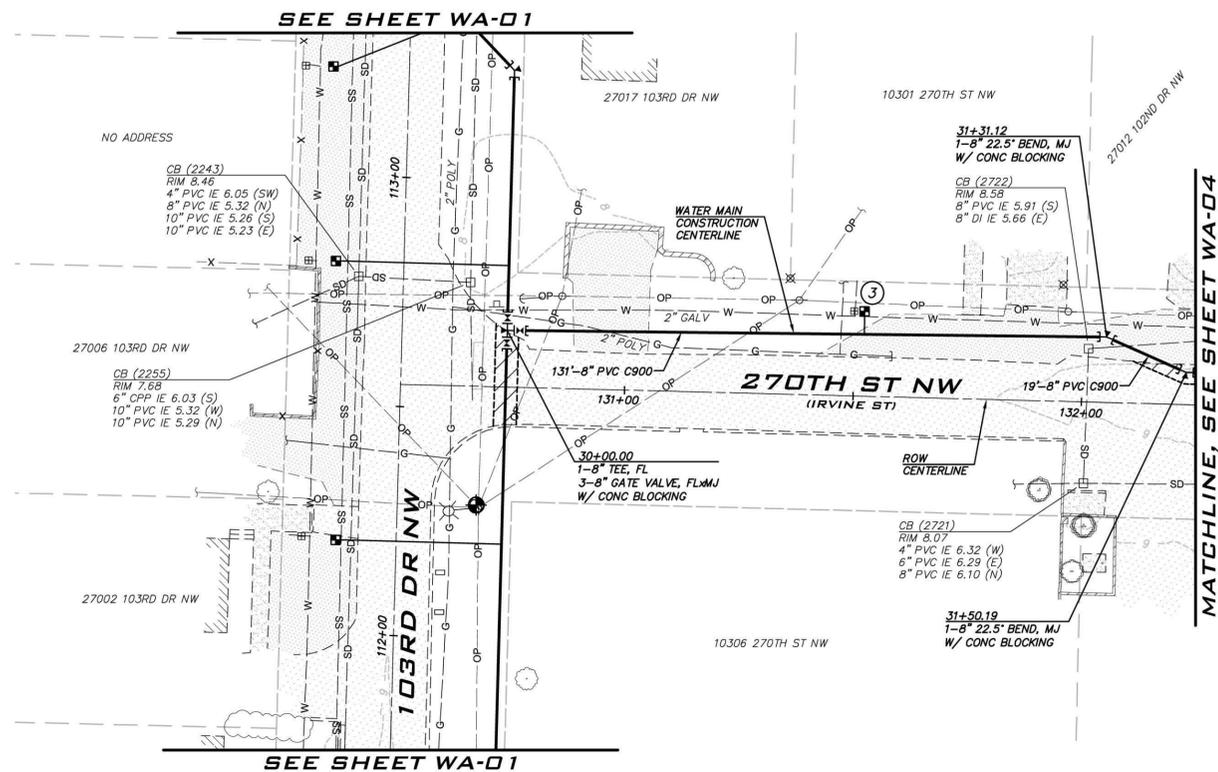
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WATER PLAN & PROFILE
 103RD DR NW
 IMPROVEMENTS PROJECT
 PHASE II
 CITY OF STANWOOD WASHINGTON



4/1/25
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SCALE: AS NOTED

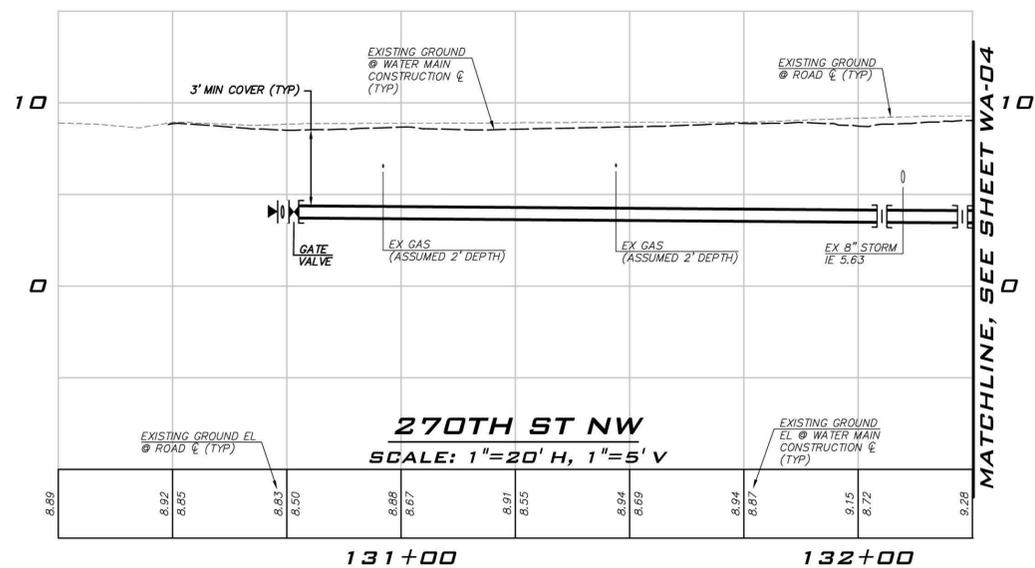
PROJECT MANAGER:
BRETT JUSTINEN, PE

PROJECT ENGINEER:
EVAN BOVARD, EIT

DESIGNER:
CAROL DOGGETT

ISSUE DATE:
4/1/2025

NO	DATE	BY	REVISIONS



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WATER PLAN & PROFILE
 103RD DR NW
 IMPROVEMENTS PROJECT
 PHASE II
 CITY OF STANWOOD WASHINGTON

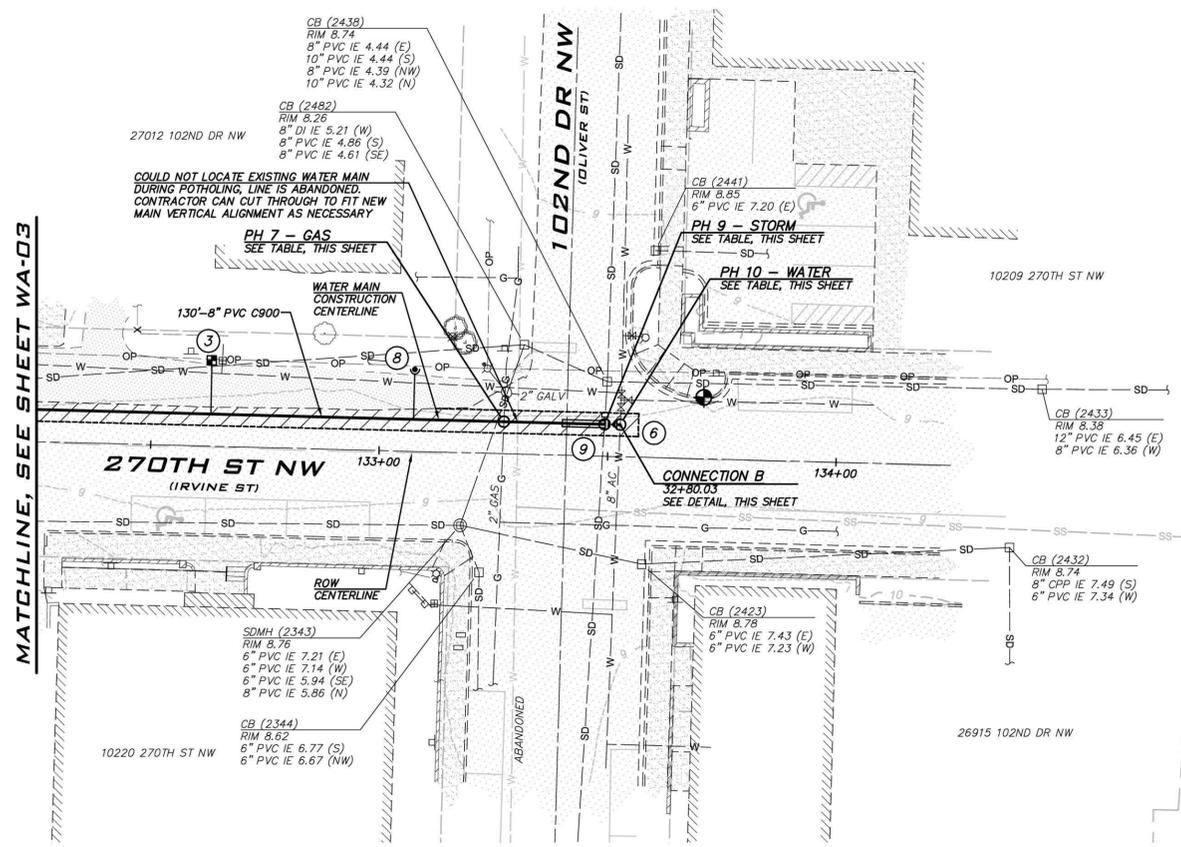
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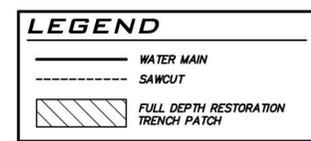
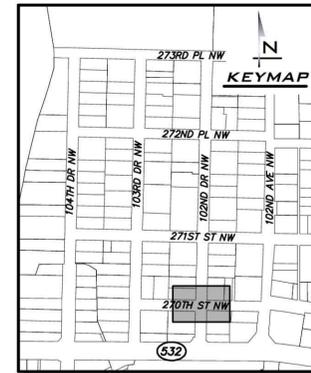
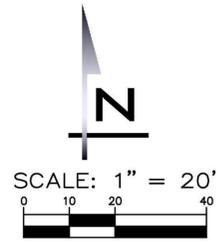
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SHT **8** OF **17**

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MATCHLINE, SEE SHEET WA-03



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SCALE: AS NOTED

PROJECT MANAGER:
BRETT JUSTINEN, PE

PROJECT ENGINEER:
EVAN BOYARD, EIT

DESIGNER:
CAROL DOGGETT

ISSUE DATE:
4/1/2025

POTHOLE DATA

POTHOLE #	TARGET UTILITY	PIPE CONDUIT SIZE (INCHES)	PIPE MATERIAL	DEPTH TO TOP OF UTILITY (INCHES)	DEPTH TO BOTTOM OF UTILITY (INCHES)	ASPHALT THICKNESS
7	GAS	2"	UNKNOWN	30"	32"	ASSUMED 2"
8	NOT USED					
9	STORM	10"	PVC	48"	59"	ASSUMED 2"
10	WATER	8"	AC	33"	57" ASSUMED	ASSUMED 2"

CONSTRUCTION NOTES

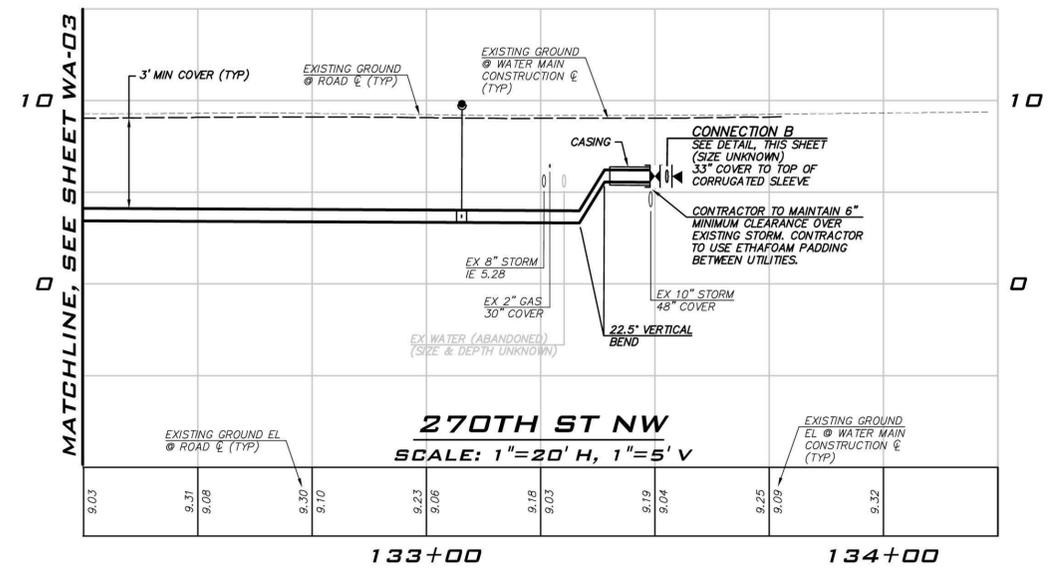
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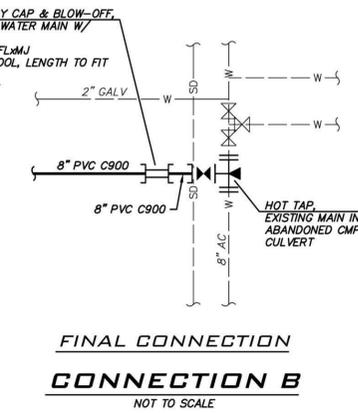
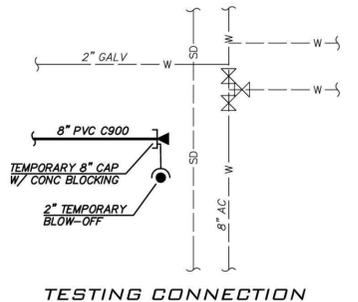
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MATCHLINE, SEE SHEET WA-03



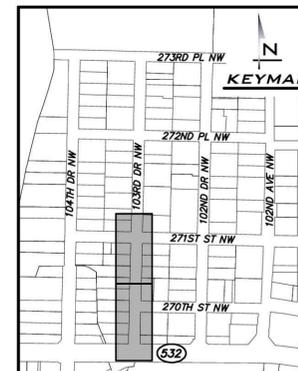
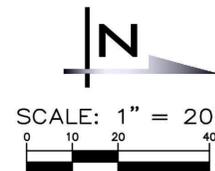
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WATER PLAN & PROFILE
103RD DR NW
IMPROVEMENTS PROJECT
PHASE II
CITY OF STANWOOD WASHINGTON



4/1/25
 JOB NUMBER:
23006268
 SHEET NAME:
WA-04
 SHT **9** OF **17**

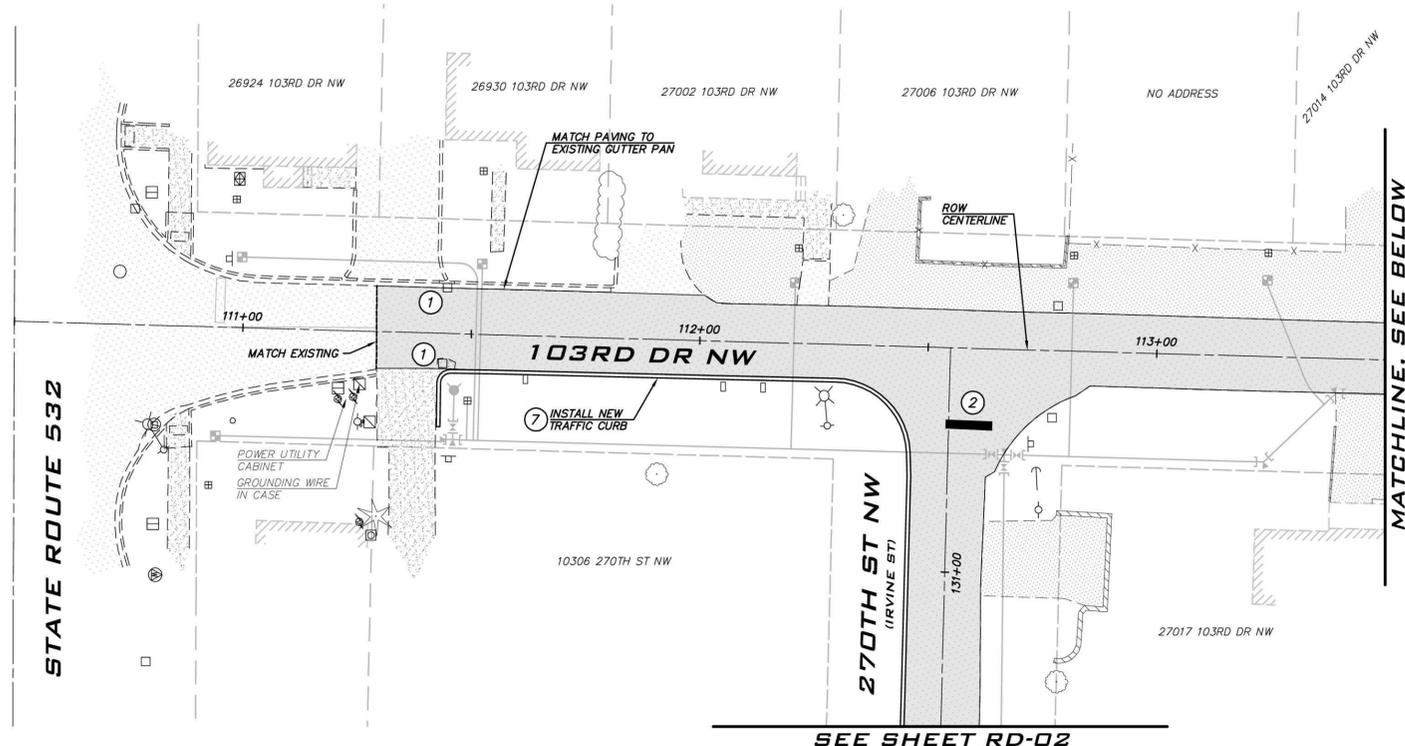
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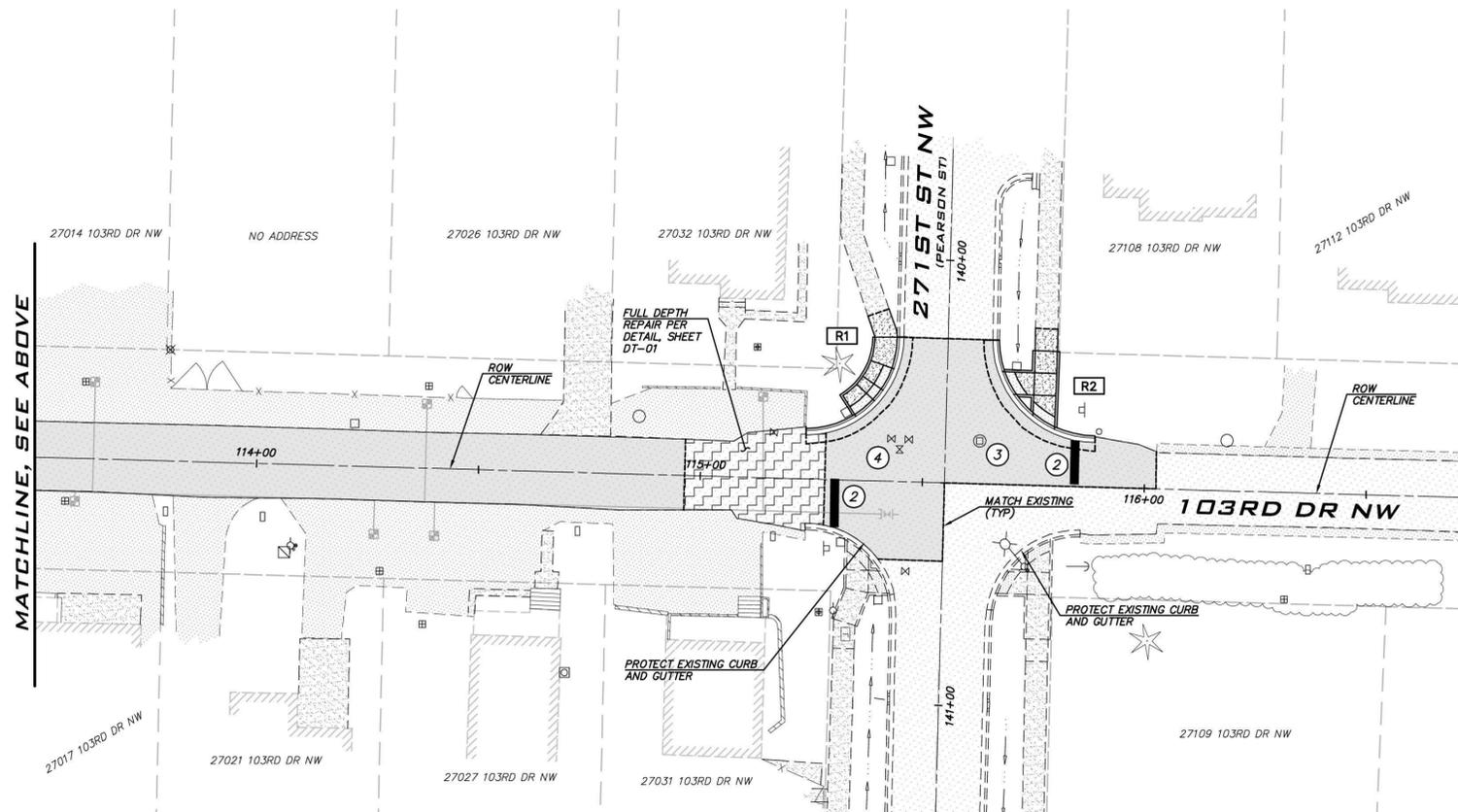
SCALE: AS NOTED
 PROJECT MANAGER: BRETT JUSTINEN, PE
 PROJECT ENGINEER: EVAN BOYARD, EIT
 DESIGNER: CAROL DOGGETT
 ISSUE DATE: 4/1/2025

LEGEND

- 3" GRIND & OVERLAY
- FULL-DEPTH REPAIR
- SAWCUT
- DASHED YELLOW CENTERLINE
- SOLID WHITE LINE
- STOP BAR
- HANDICAP PARKING SYMBOL



SEE SHEET RD-02



NOTES

- CURB RAMPS SHALL BE INSTALLED AS SHOWN ON SHEET CR-01.
- PAVING EXTENTS SHOWN ARE APPROXIMATE. CITY ENGINEER TO DETERMINE FINAL EXTENTS OF ROAD REHABILITATION LIMITS.

CONSTRUCTION NOTES

- ADJUST CATCH BASIN FRAME & GRATE IF NECESSARY.
- INSTALL PLASTIC STOP BAR PER SNOHOMISH COUNTY DETAIL 7-100.
- ADJUST MANHOLE TO GRADE IF NECESSARY.
- ADJUST WATER VALVE BOX TO GRADE IF NECESSARY.
- INSTALL 4" WHITE EDGE LINE PER CITY OF STANWOOD DETAIL T-9.
- INSTALL HANDICAPPED PARKING STALL SYMBOL PER SNOHOMISH COUNTY DETAIL 7-140.
- INSTALL CEMENT CONC. TRAFFIC CURB PER WSDOT STD PLAN F-10.12-04. ALSO SEE OVERLAY DETAIL ON SHEET DT-01.

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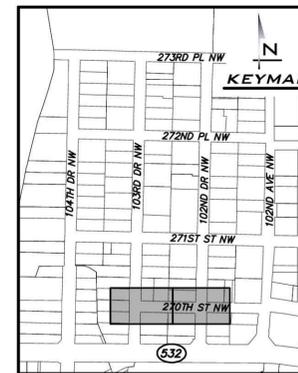
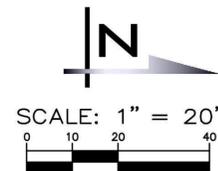
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ROAD IMPROVEMENT PLAN
 103RD DR NW
 IMPROVEMENTS PROJECT
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 CITY OF STANWOOD WASHINGTON



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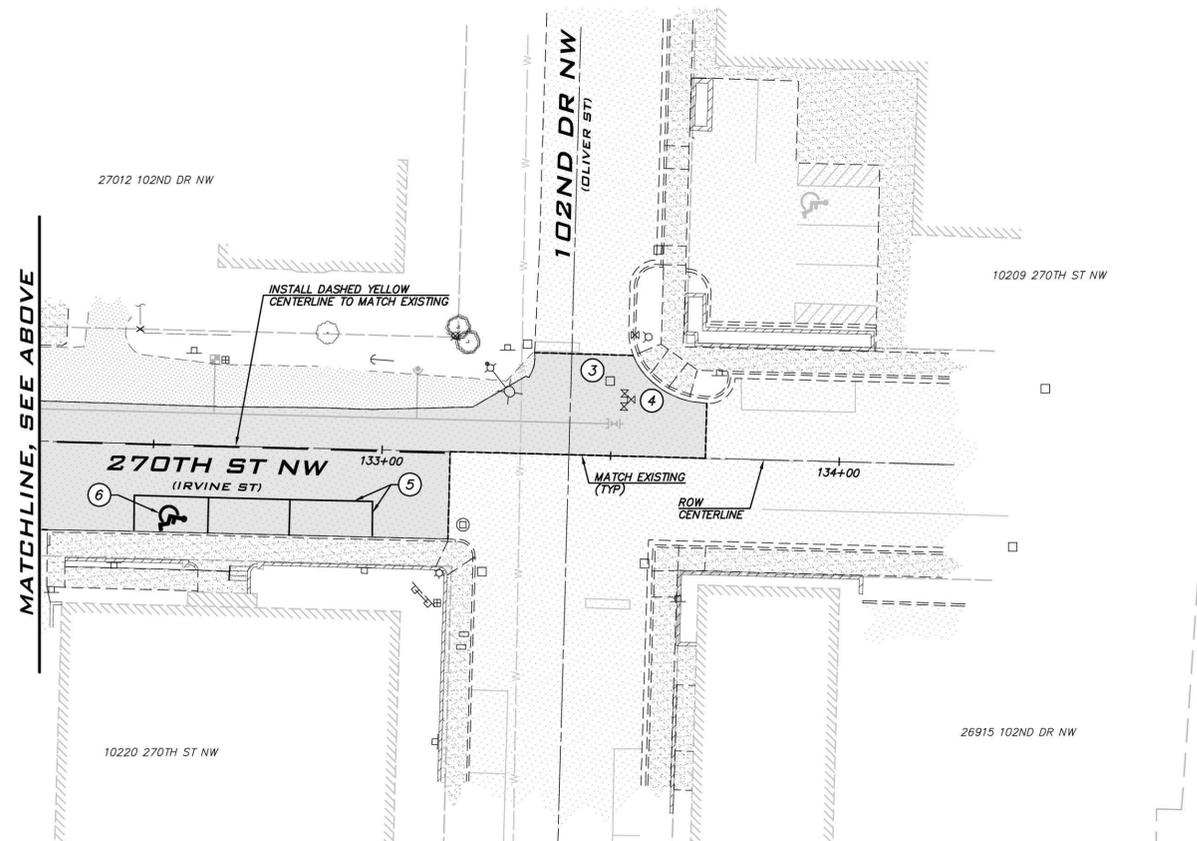
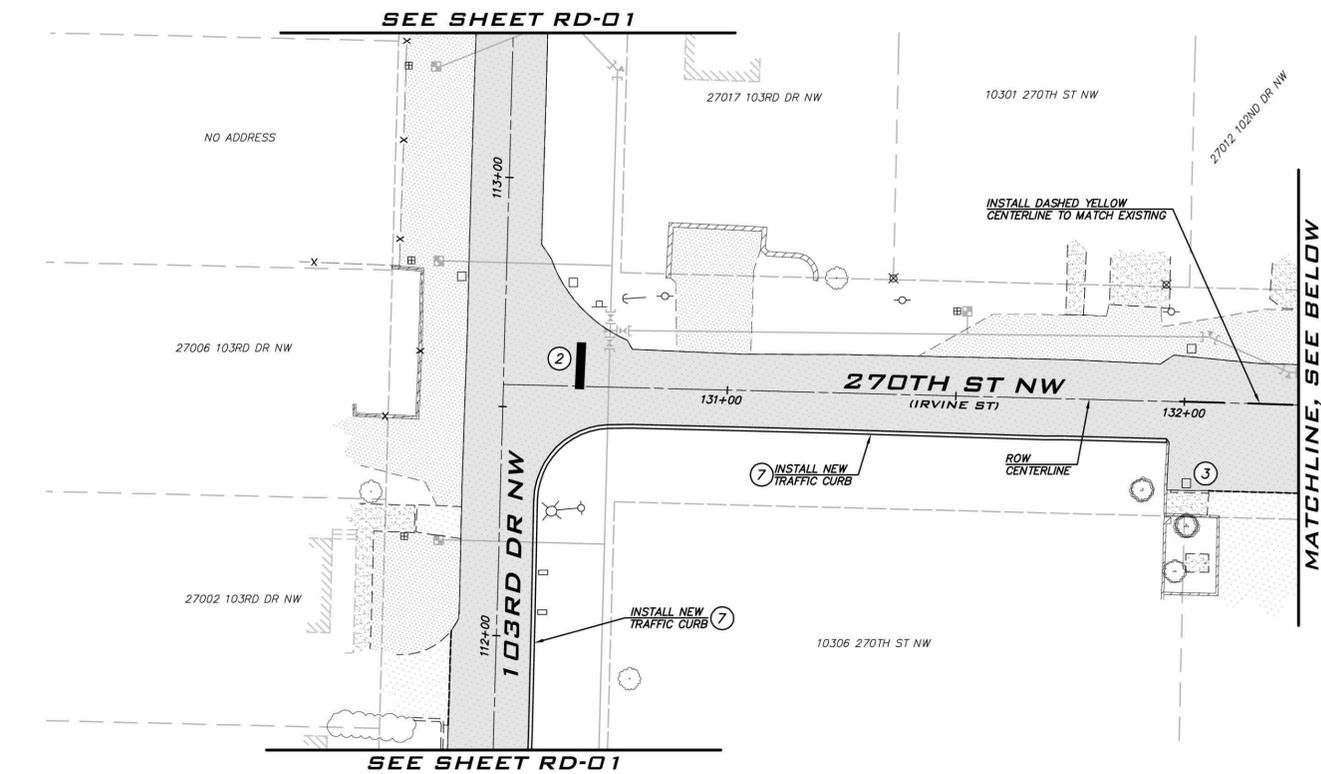


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	FULL-DEPTH REPAIR
	SAWCUT
	DASHED YELLOW CENTERLINE
	SOLID WHITE LINE
	STOP BAR
	HANDICAP PARKING SYMBOL



- NOTES**
- CURB RAMPS SHALL BE INSTALLED AS SHOWN ON SHEET CR-01.
 - PAVING EXTENTS SHOWN ARE APPROXIMATE. CITY ENGINEER TO DETERMINE FINAL EXTENTS OF ROAD REHABILITATION LIMITS.

- CONSTRUCTION NOTES**
- ADJUST CATCH BASIN FRAME & GRATE IF NECESSARY.
 - INSTALL PLASTIC STOP BAR PER SNOHOMISH COUNTY DETAIL 7-100.
 - ADJUST MANHOLE TO GRADE IF NECESSARY.
 - ADJUST WATER VALVE BOX TO GRADE IF NECESSARY.
 - INSTALL 4" WHITE EDGE LINE PER CITY OF STANWOOD DETAIL T-9.
 - INSTALL HANDICAPPED PARKING STALL SYMBOL PER SNOHOMISH COUNTY DETAIL 7-140.
 - INSTALL CEMENT CONC. TRAFFIC CURB PER WSDOT STD PLAN F-10.12-04. ALSO SEE OVERLAY DETAIL ON SHEET DT-01.

EXISTING UTILITY NOTE

EXISTING UTILITIES ARE SHOWN IN THE APPROXIMATE LOCATION. THERE IS NO GUARANTEE THAT ALL UTILITY LINES ARE SHOWN, OR THAT THE LOCATION, SIZE AND MATERIAL IS ACCURATE. THE CONTRACTOR SHALL UNCOVER ALL INDICATED PIPING WHERE CROSSING, INTERFERENCES, OR CONNECTIONS OCCUR PRIOR TO TRENCHING OR EXCAVATION FOR ANY PIPE OR STRUCTURES, TO DETERMINE ACTUAL LOCATIONS, SIZE AND MATERIAL. THE CONTRACTOR SHALL MAKE THE APPROPRIATE PROVISION FOR PROTECTION OF SAID FACILITIES. THE CONTRACTOR SHALL NOTIFY ONE CALL AT 8-1-1 (WASHINGTON811.COM) AND ARRANGE FOR FIELD LOCATION OF EXISTING FACILITIES BEFORE CONSTRUCTION.

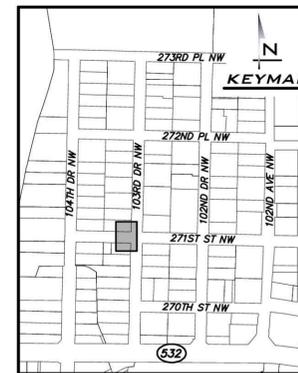
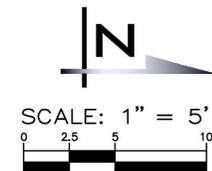
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ROAD IMPROVEMENT PLAN
 103RD DR NW
 IMPROVEMENTS PROJECT
 PHASE II
 CITY OF STANWOOD WASHINGTON



4/1/25
JOB NUMBER:
23006268
SHEET NAME:
RD-02

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SCALE:
AS NOTED

PROJECT MANAGER:

BRETT JUSTINEN, PE

PROJECT ENGINEER:

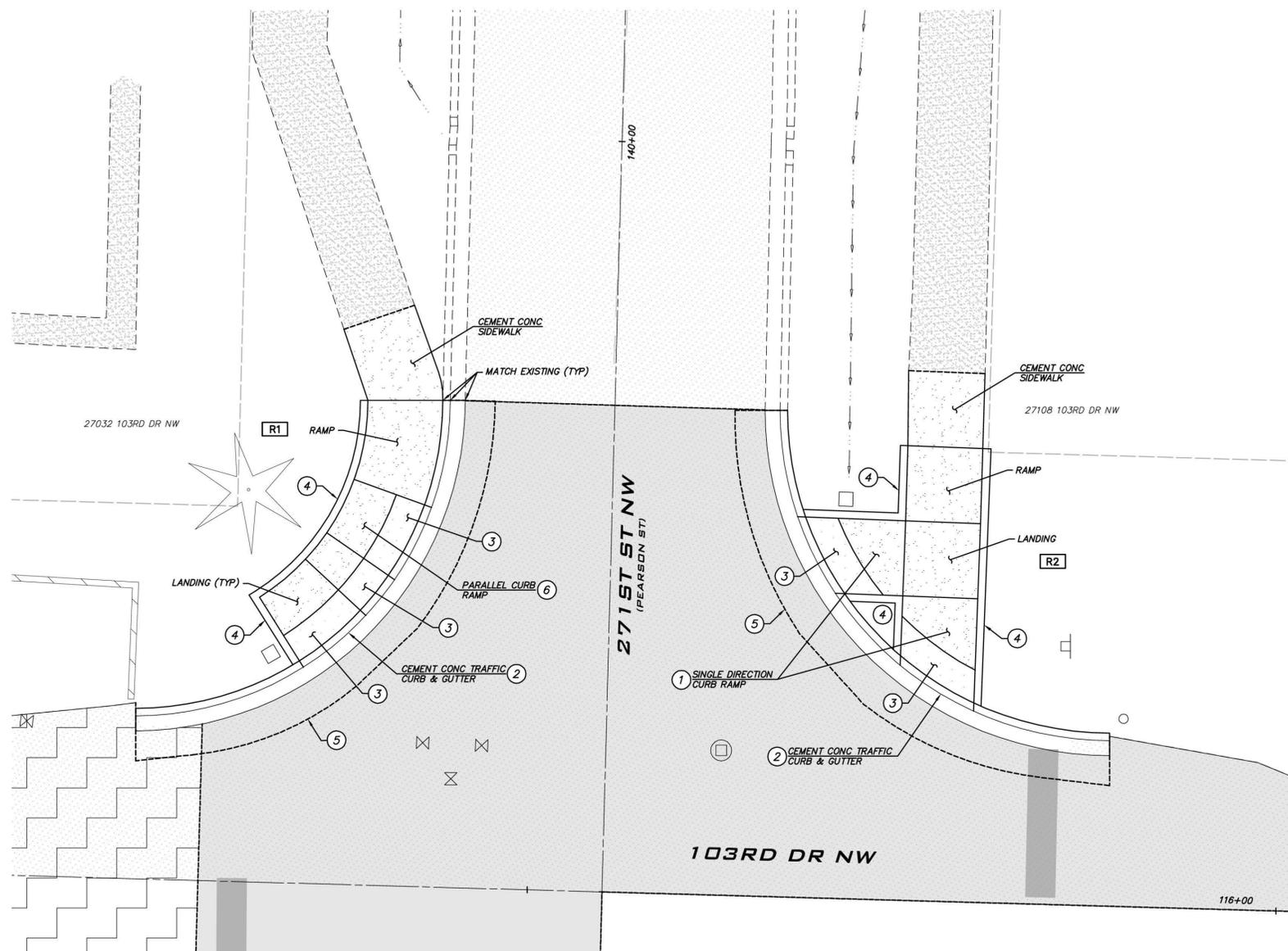
EVAN BOYARD, EIT

DESIGNER:

CAROL DOGGETT

ISSUE DATE:

4/1/2025



- NOTES**
1. ALL SIDEWALKS, CURB RAMPS, AND PEDESTRIAN CURBS SHALL BE CONSTRUCTED OF CLASS 3000 CONCRETE.
 2. FORM AND SUBGRADE INSPECTION BY THE CITY OR ENGINEER ARE REQUIRED BEFORE CEMENT CONCRETE IS POURED.
 3. MONOLITHIC POUR OF CURB, GUTTER AND SIDEWALK WILL NOT BE ALLOWED, EXCEPT IN PEDESTRIAN CURB RAMP AREAS.

- CONSTRUCTION NOTES**
1. INSTALL MODIFIED SINGLE DIRECTION CURB RAMP, TYPE A PER WSDOT STD PLAN F-40.16-03.
 2. INSTALL CEMENT CONCRETE TRAFFIC CURB AND GUTTER PER WSDOT STD PLAN F-10.12-04. CLASS 4000 CONCRETE.
 3. INSTALL DETECTABLE WARNING SURFACE PER WSDOT STD PLAN F-45.10-04.
 4. INSTALL CEMENT CONCRETE PEDESTRIAN CURB PER WSDOT STD PLAN F-10.12-04. PEDESTRIAN CURB MAY BE OMITTED AT CURB RAMPS WHERE THERE IS NO EARTH TO RETAIN. REFER TO CURB RAMP STANDARD PLANS AND COORDINATE WITH CITY INSPECTOR.
 5. SAW CUT LIMIT 2-FOOT MINIMUM BEYOND PROPOSED FACE OF GUTTER PER DETAIL ON SHEET DT-01. CONTRACTOR TO ENSURE PROPER COMPACTION CAN BE ACHIEVED IN REMOVED WIDTH.
 6. INSTALL PARALLEL CURB RAMP PER WSDOT STD PLAN F-40.12-03.

EXISTING UTILITY NOTE

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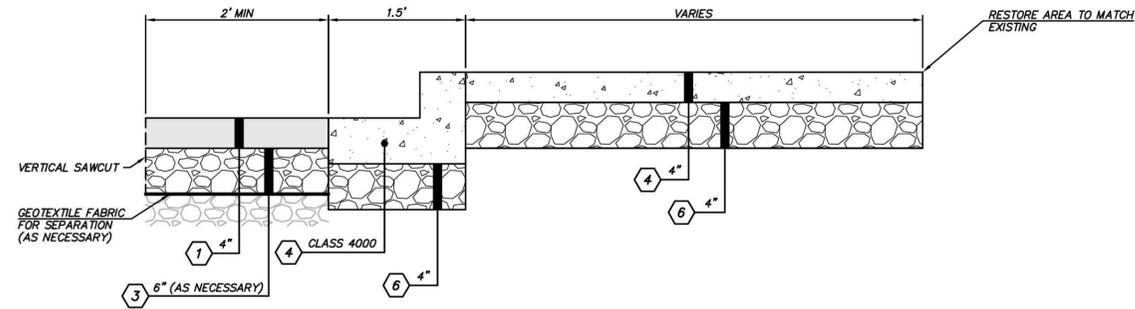
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CURB RAMP DETAILS
103RD DR NW
IMPROVEMENTS PROJECT
PHASE II
CITY OF STANWOOD WASHINGTON

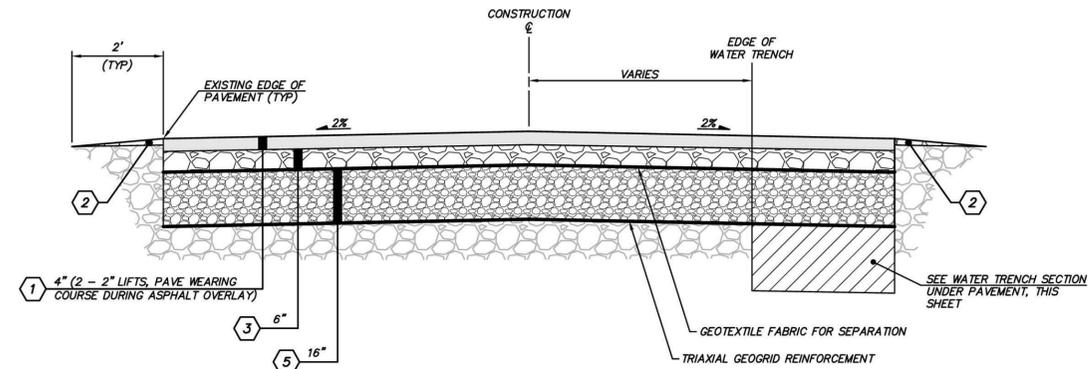


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CR-01

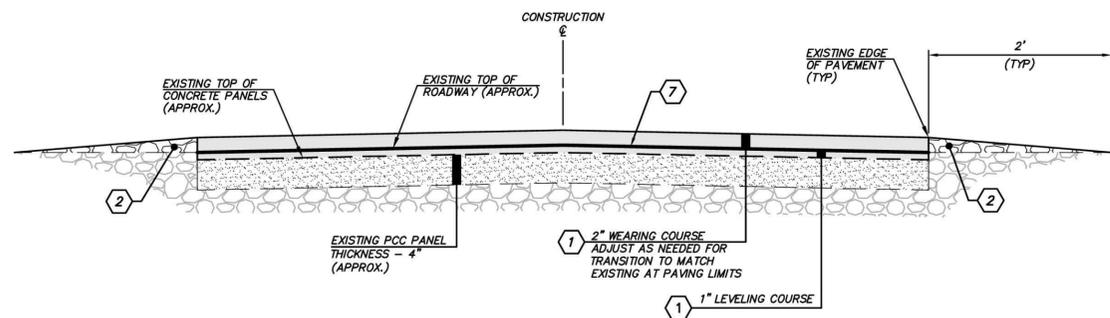
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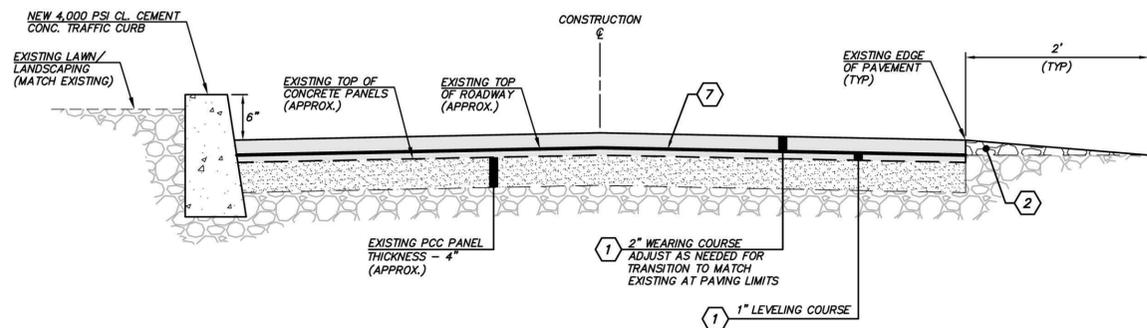
CURB RAMP / SIDEWALK DETAIL
NOT TO SCALE



SECTION A-A FULL DEPTH RECONSTRUCTION
NOT TO SCALE



ASPHALT OVERLAY DETAIL
NOT TO SCALE



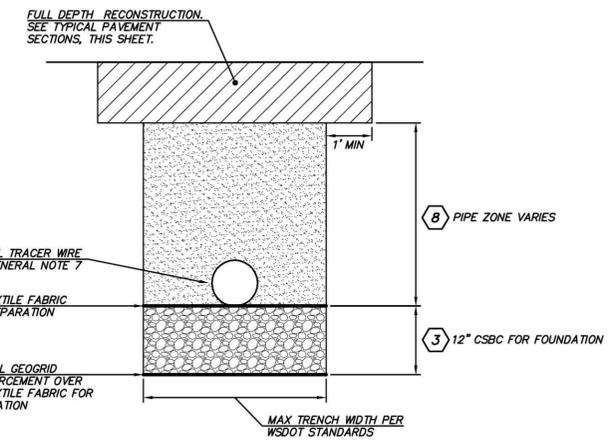
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NOTES

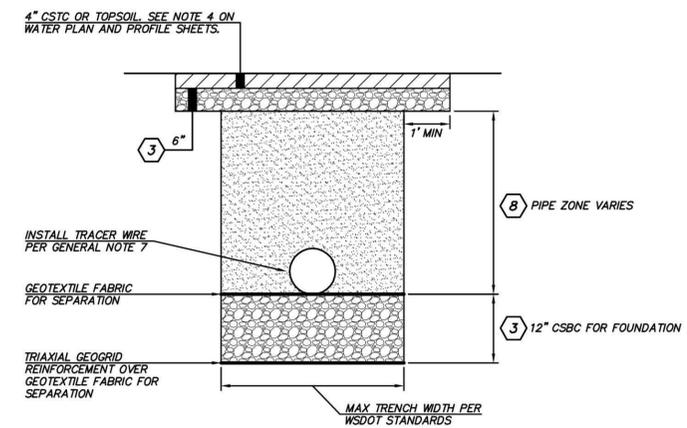
- ① HMA CL. 1/2" PG 58H-22. 2% MINIMUM CROSS SLOPE.
- ② INSTALL CSTC FOR SHOULDER AS NEEDED TO SUPPORT EDGE OF PAVEMENT AFTER PAVING. 0-2" TAPER OR AS DIRECTED BY THE ENGINEER. MINIMUM COMPACTION SHALL BE 90% OF MAXIMUM DENSITY.
- ③ CSBC FOR ROAD BASE. THE MINIMUM COMPACTION SHALL BE 95% OF MAXIMUM DENSITY.
- ④ CEMENT CONCRETE, CLASS 3000 UNLESS OTHERWISE NOTED BELOW.
- ⑤ PERMEABLE BALLAST PER WSDOT SECTION 9-03.9(2).
- ⑥ CSTC FOR CONCRETE BASE. THE MINIMUM COMPACTION SHALL BE 95% OF MAXIMUM DENSITY.
- ⑦ ASPHALT INTERLAYER REINFORCEMENT. TENSAR GLASGRID 8511 OR APPROVED EQUAL.
- ⑧ LIGHTWEIGHT BACKFILL, SAND ($\gamma = 100-105$ PCF) OR CDF IF COVER IS UNDER 36" MINIMUM.

LEGEND

- DEPTH OF GRIND
- [Pattern] HMA
- [Pattern] CSTC/CSBC
- [Pattern] EXISTING SUBGRADE
- [Pattern] CEMENT CONCRETE
- [Pattern] PERMEABLE BALLAST
- [Pattern] SAND



WATER TRENCH SECTION UNDER PAVEMENT
NOT TO SCALE



WATER TRENCH SECTION IN LANDSCAPE OR GRAVEL AREAS
NOT TO SCALE



SCALE: AS NOTED
 PROJECT MANAGER: BRETT JUSTINEN, PE
 PROJECT ENGINEER: EVAN BOYARD, EIT
 DESIGNER: CAROL DOGGETT
 ISSUE DATE: 4/1/2025

NO	DATE	BY	REVISIONS

DETAILS & SECTIONS
 103RD DR NW
 IMPROVEMENTS PROJECT
 PHASE II
 CITY OF STANWOOD WASHINGTON



4/1/25
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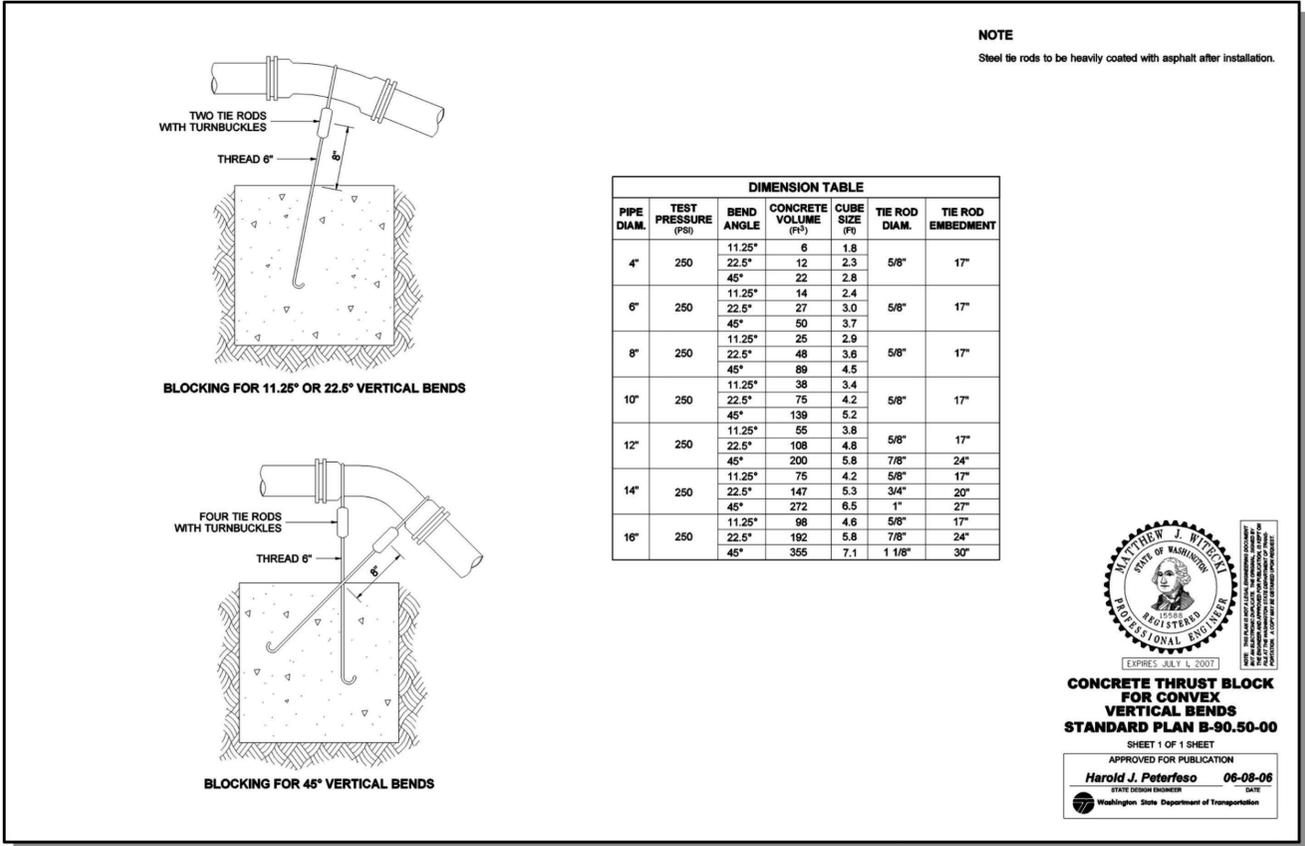
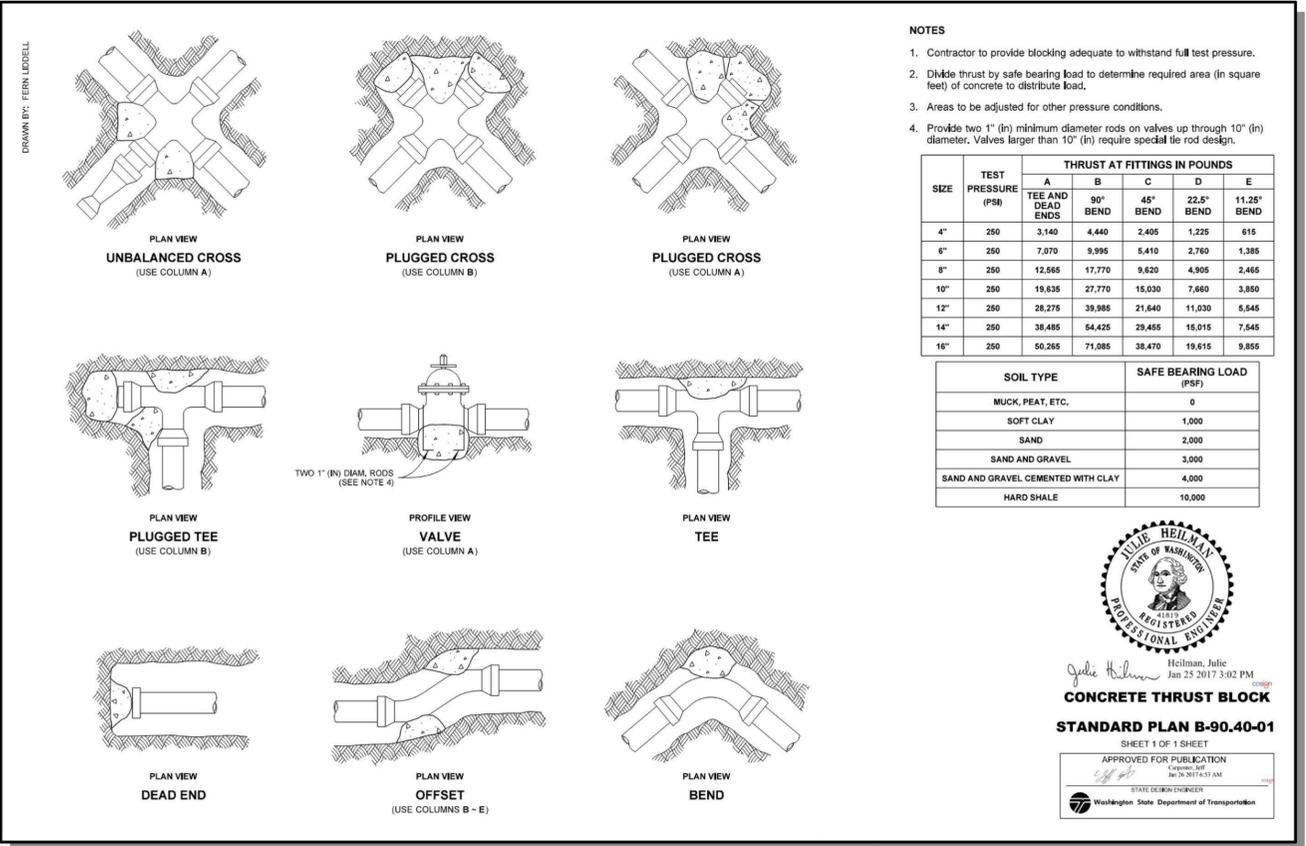
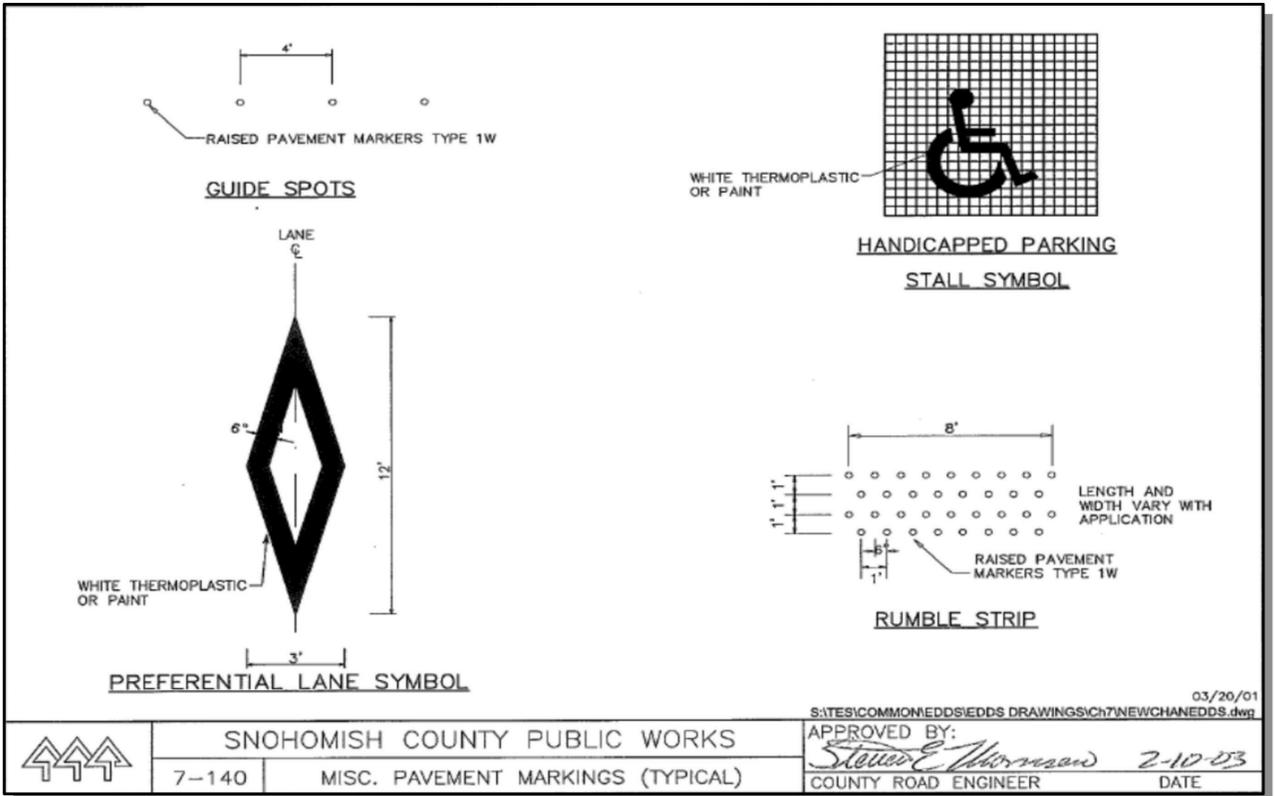
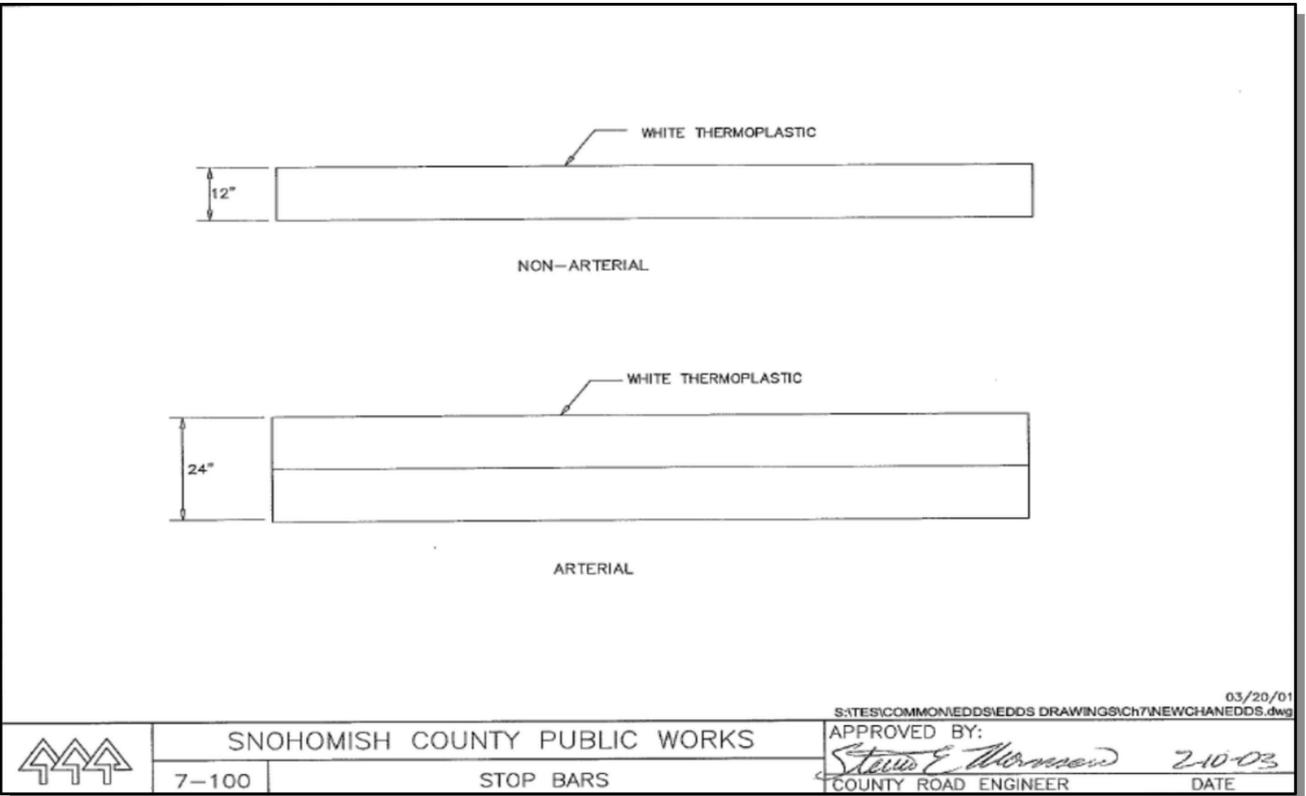
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SCALE:
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PROJECT MANAGER:
BRETT JUSTINEN, PE
PROJECT ENGINEER:
EVAN BOYARD, ET
DESIGNER:
CAROL DOGGETT
ISSUE DATE:
4/1/2025

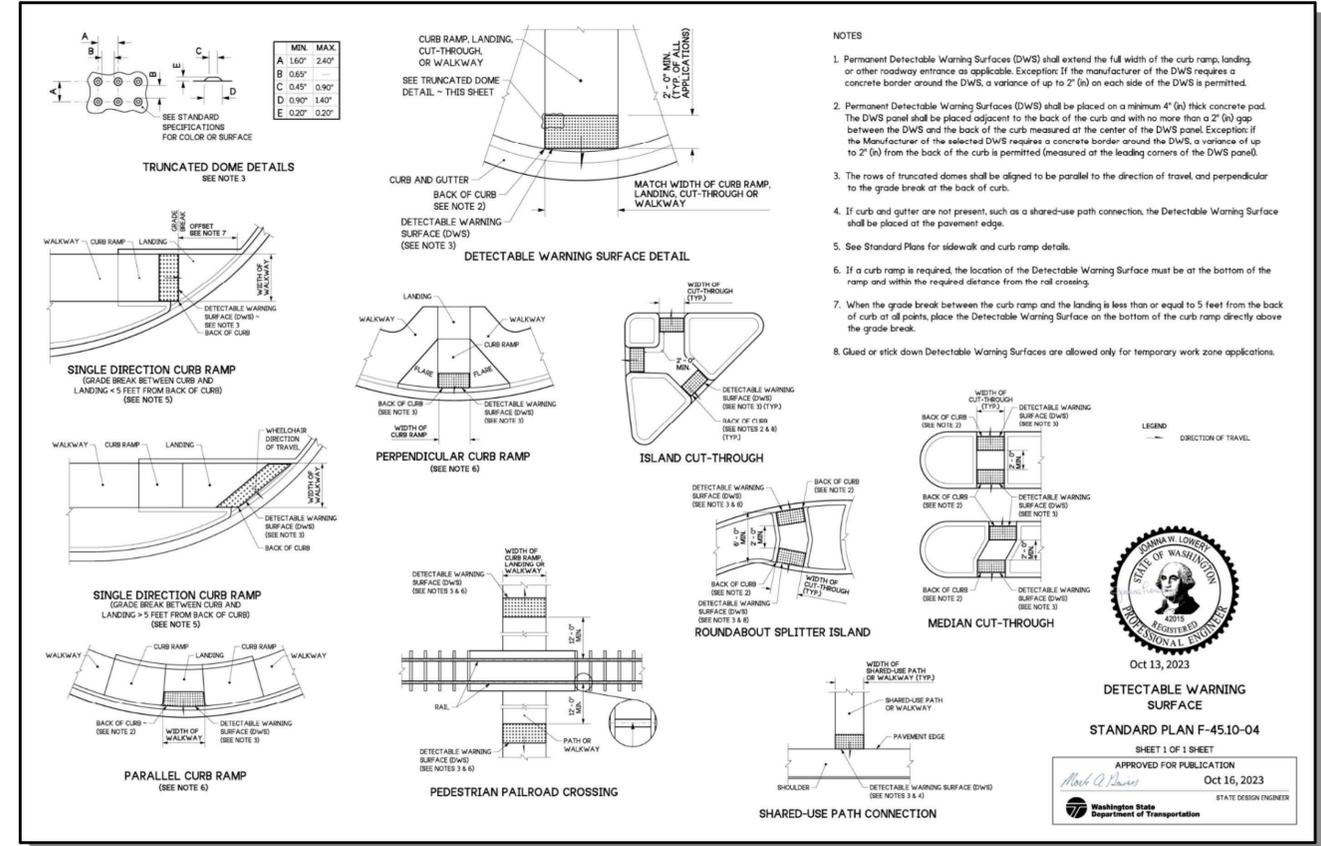
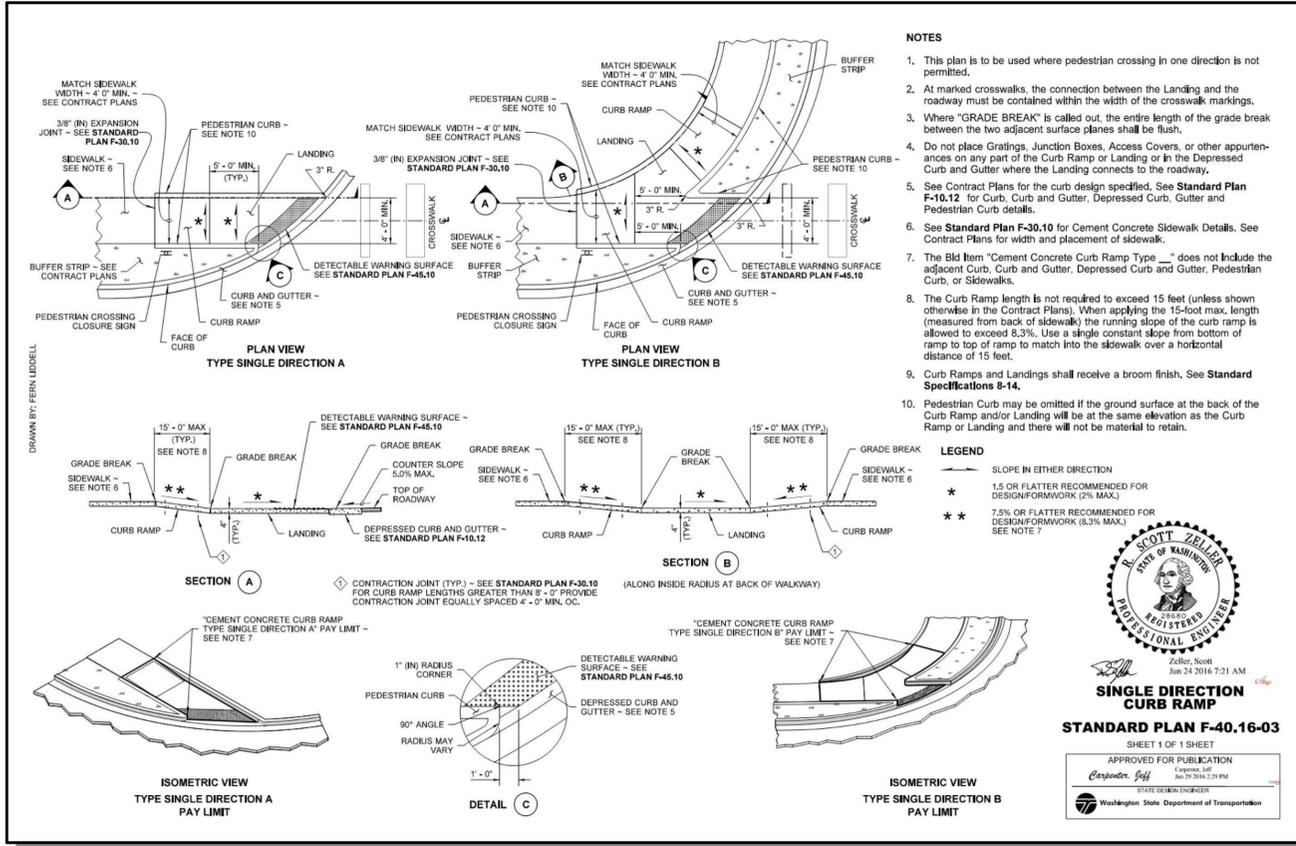
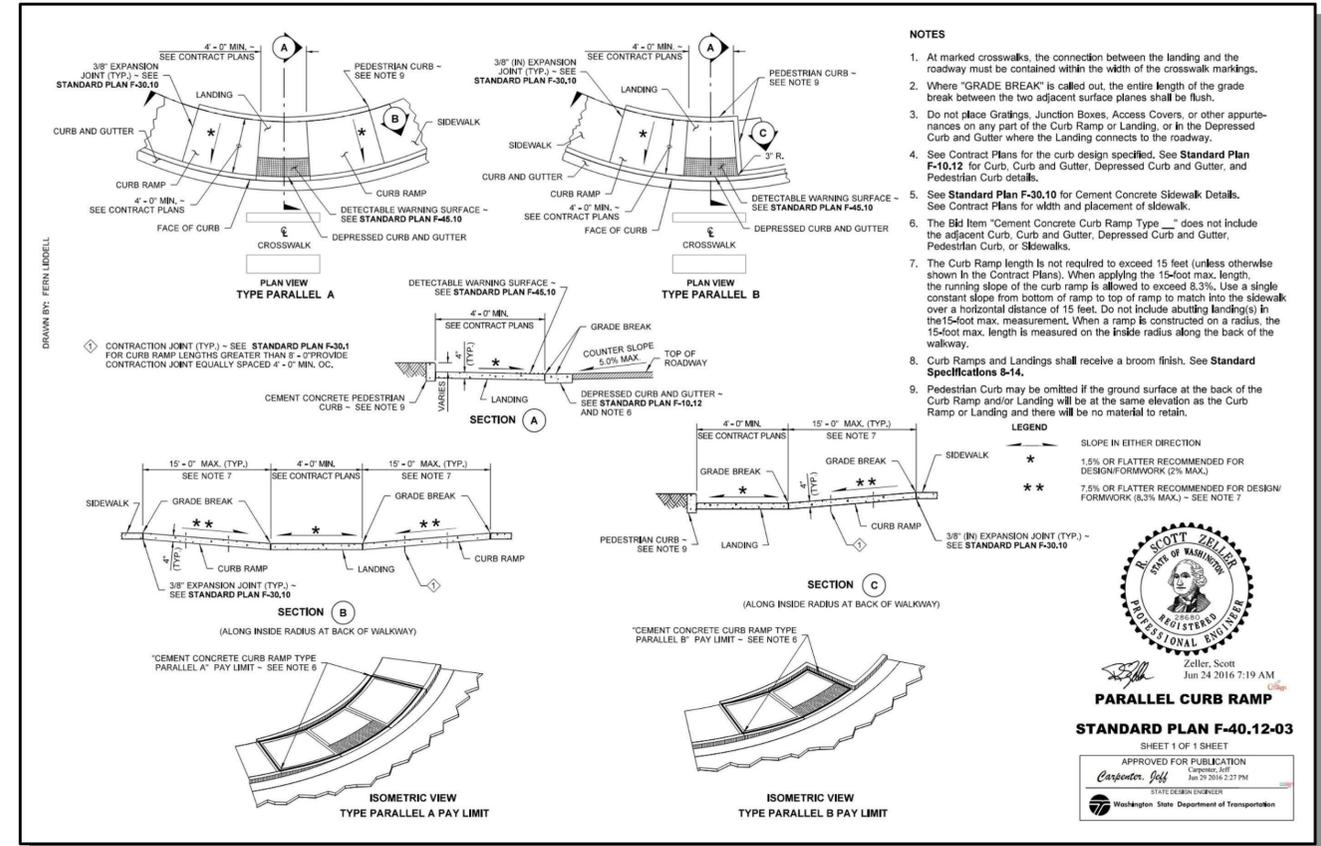
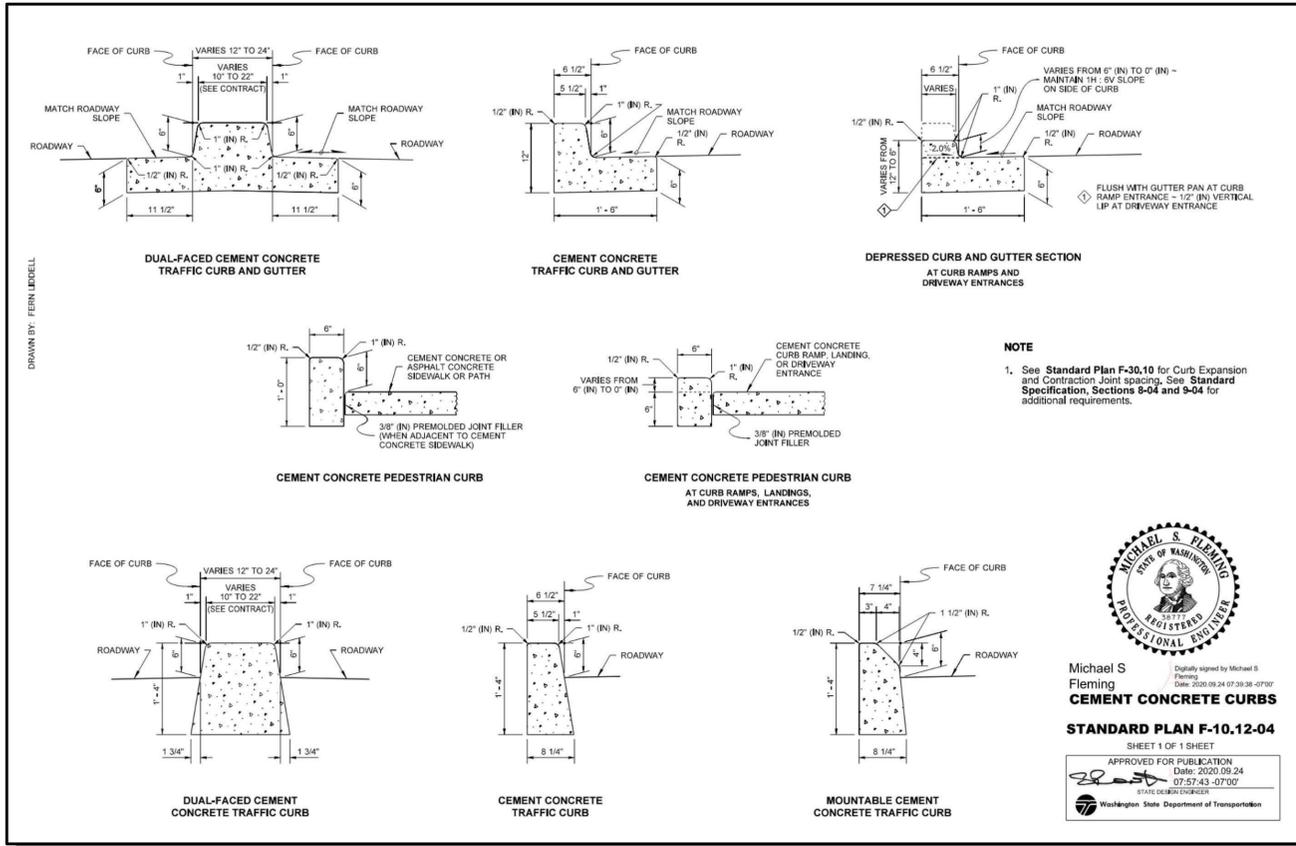
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STANDARD DETAILS
103RD DR NW
IMPROVEMENTS PROJECT
PHASE II
CITY OF STANWOOD WASHINGTON

4/1/25
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23006268
SHEET NAME:
DT-02
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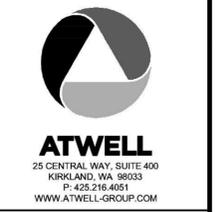
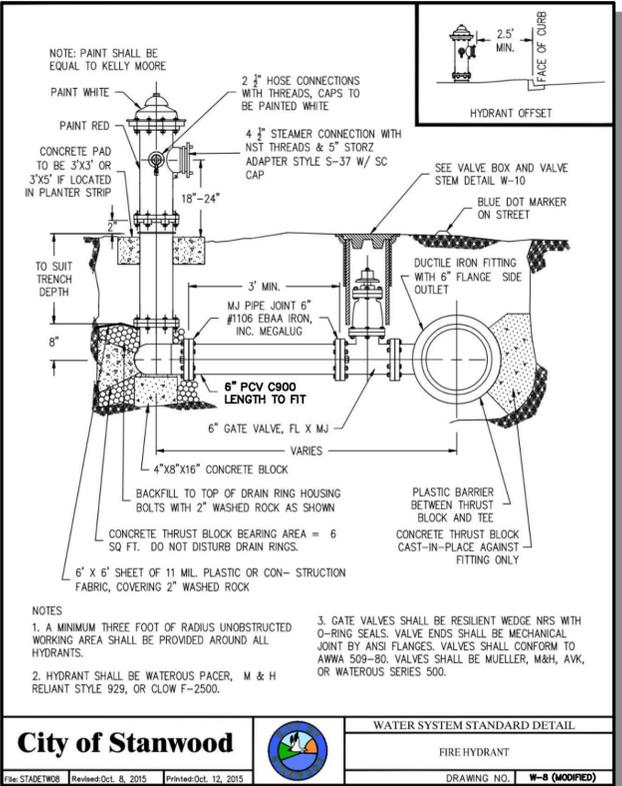
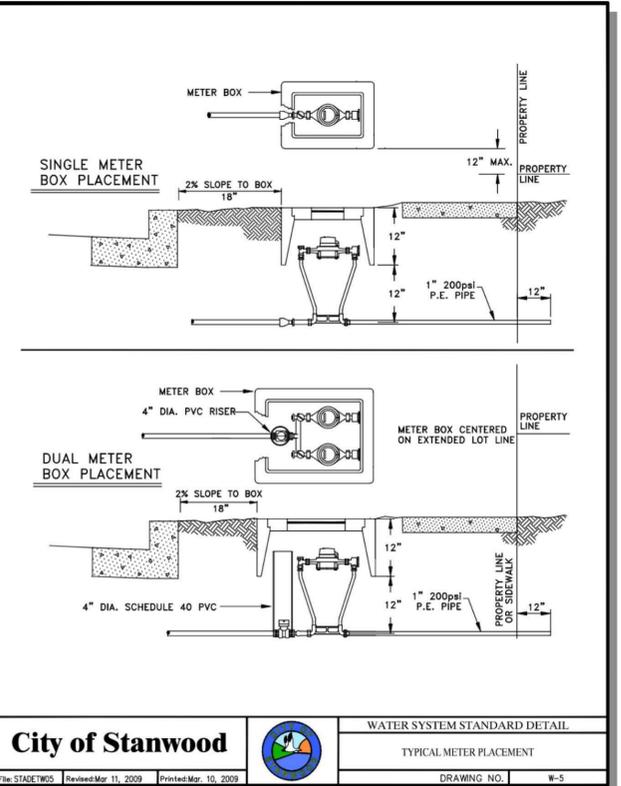
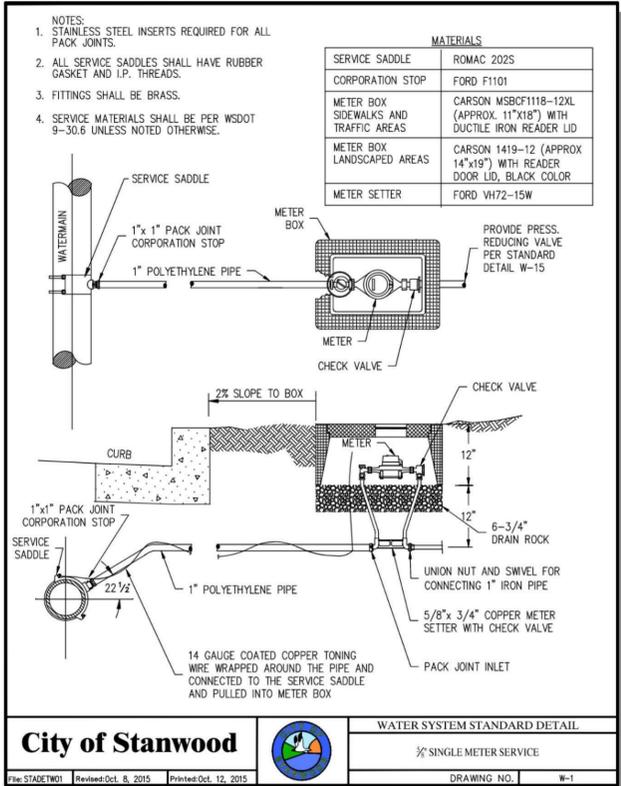
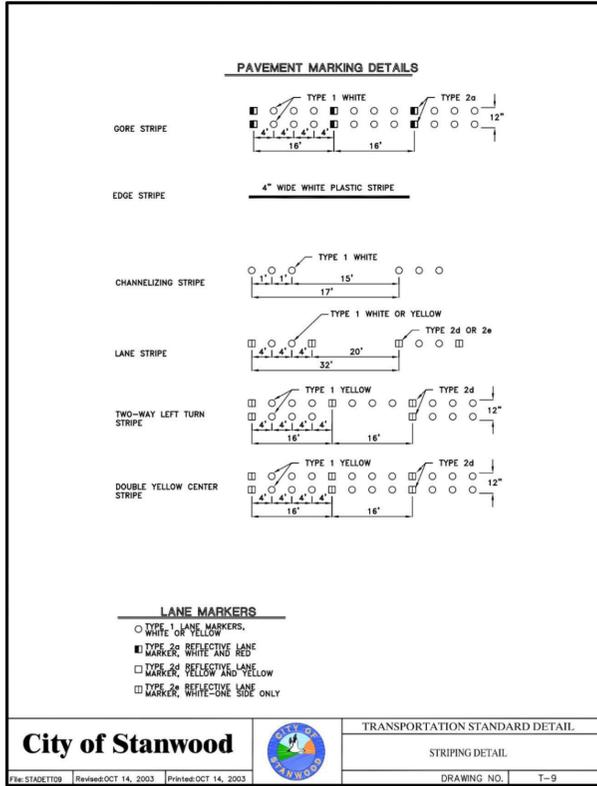
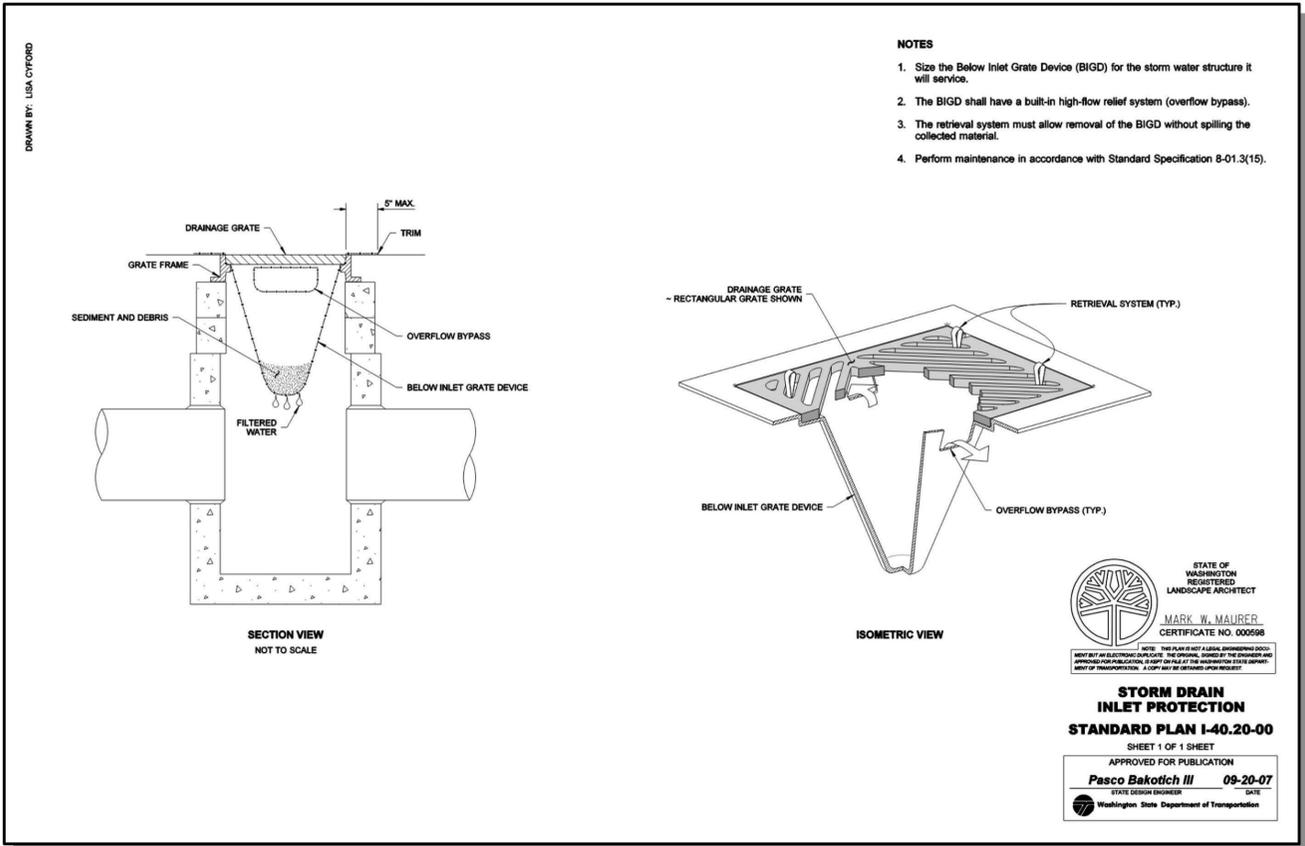
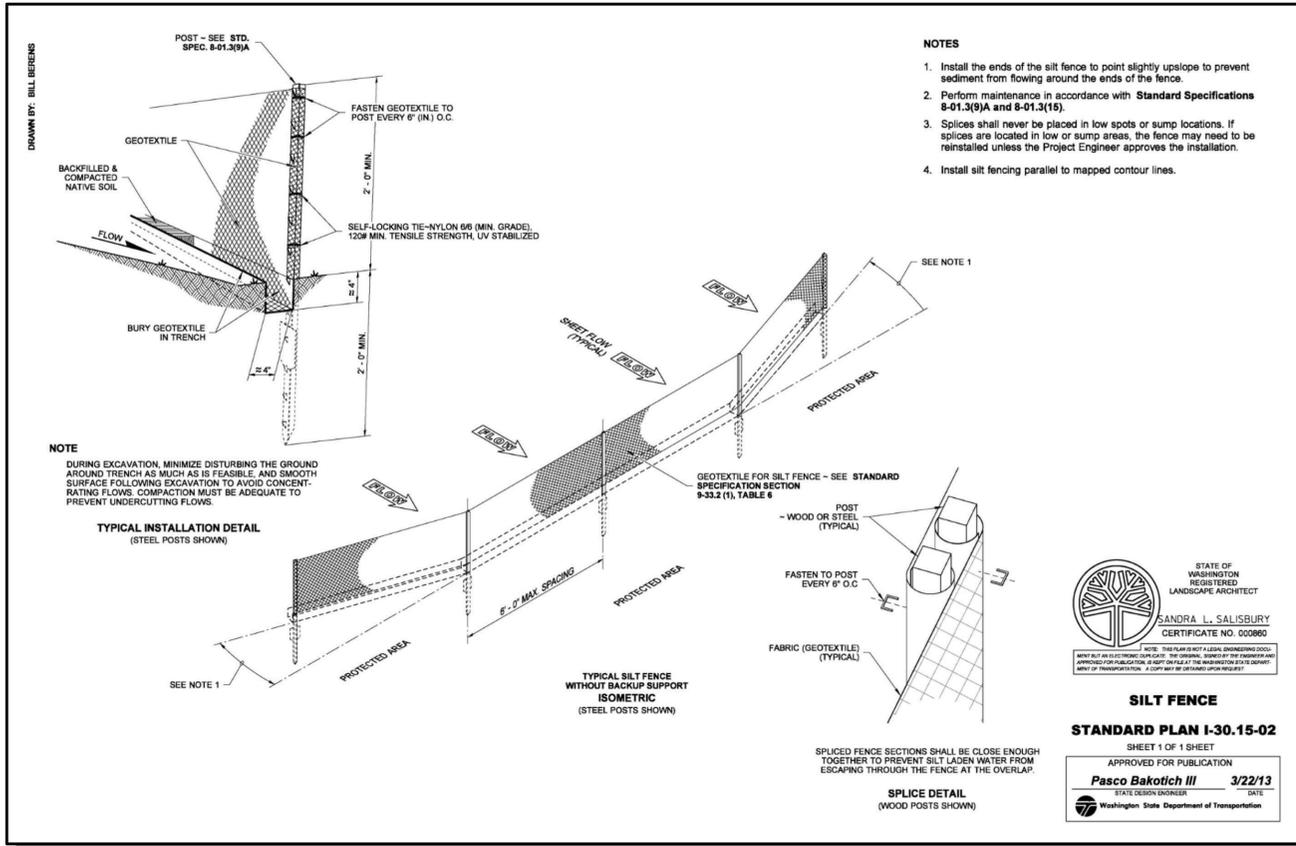
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PROJECT MANAGER: BRETT JUSTINEN, PE
PROJECT ENGINEER: EVAN BOYARD, ET
DESIGNER: CAROL DOGGETT
ISSUE DATE: 4/1/2025

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PROJECT MANAGER:
BRETT JUSTINEN, PE

PROJECT ENGINEER:
EVAN BOYARD, ET

DESIGNER:
CAROL DOGGETT

ISSUE DATE:
4/1/2025

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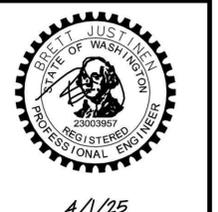
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103RD DR NW

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CITY OF STANWOOD WASHINGTON



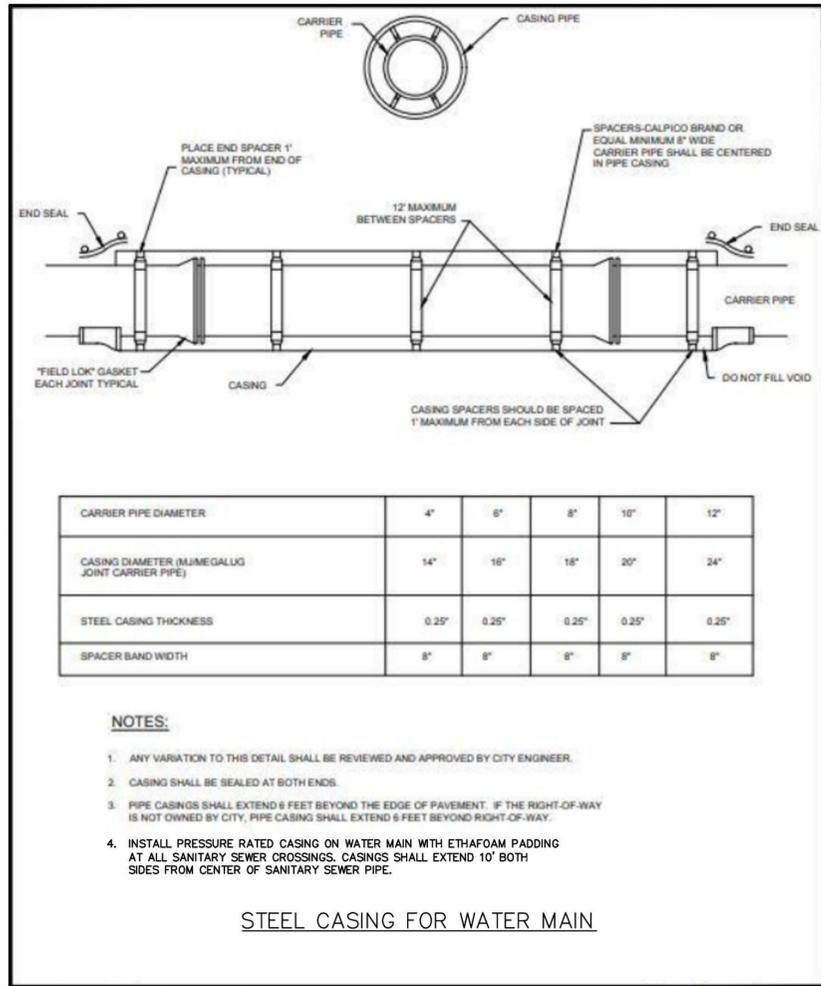
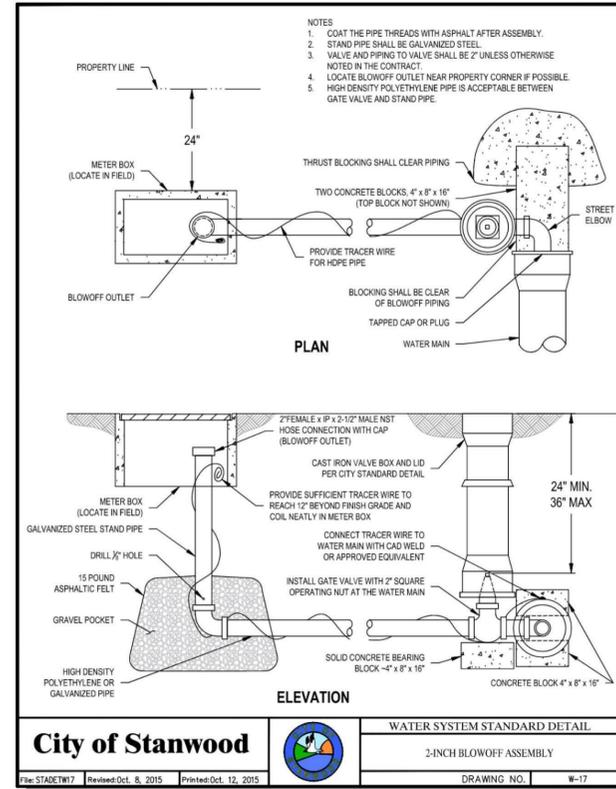
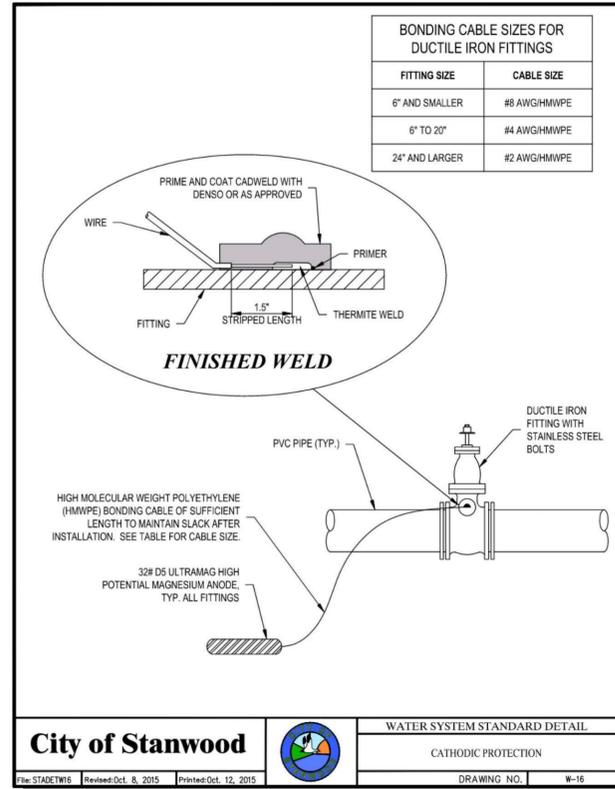
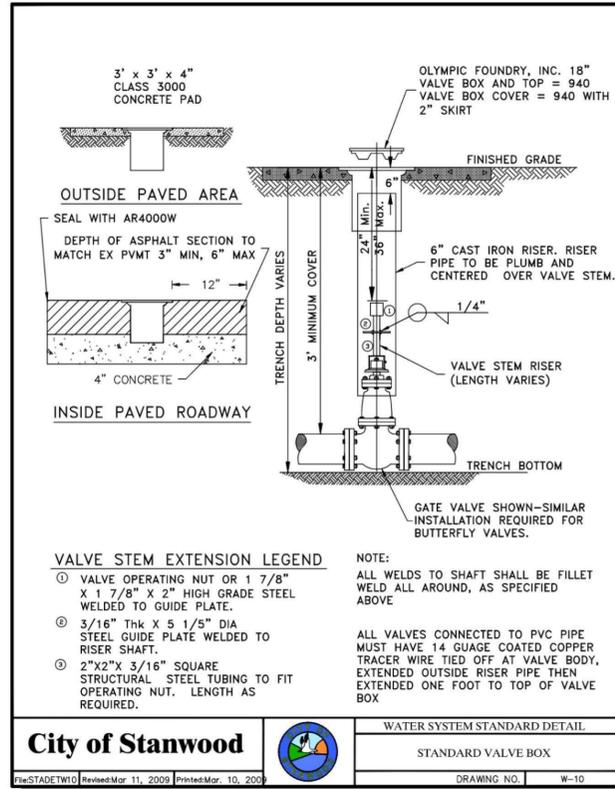
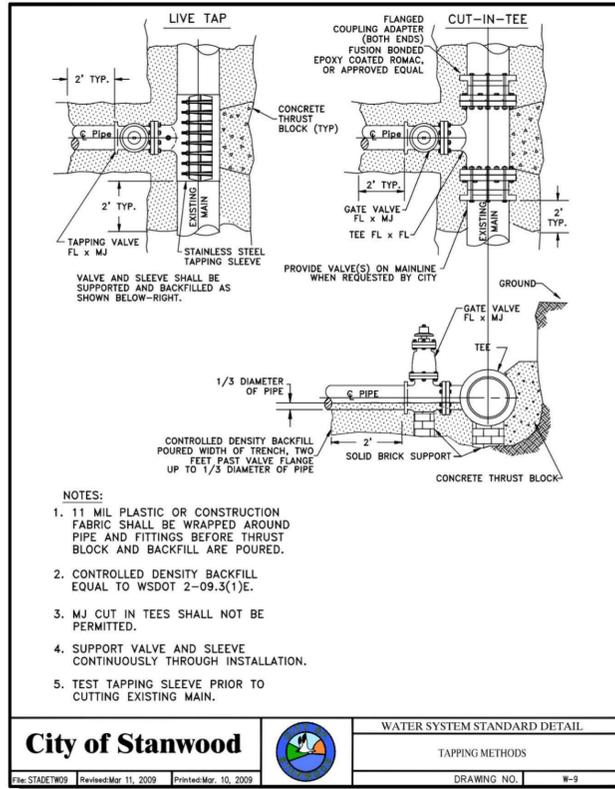
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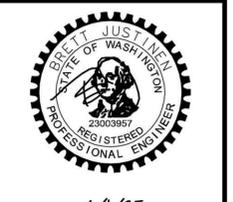
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PROJECT ENGINEER: EVAN BOYARD, ET
DESIGNER: CAROL DOGGETT
ISSUE DATE: 4/1/2025

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STANDARD DETAILS
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